

# PRELIMINARY SP PLANS

# 1711 BROADWAY

1711 BROADWAY  
NASHVILLE, TN 37203

PRELIMINARY SP SUBMITTAL: MARCH 30, 2022  
PRELIMINARY SP RESUBMITTAL: APRIL 20, 2022  
PRELIMINARY SP RESUBMITTAL: MAY 17, 2022

PRELIMINARY SP CASE NO.: 2022SP-027-001

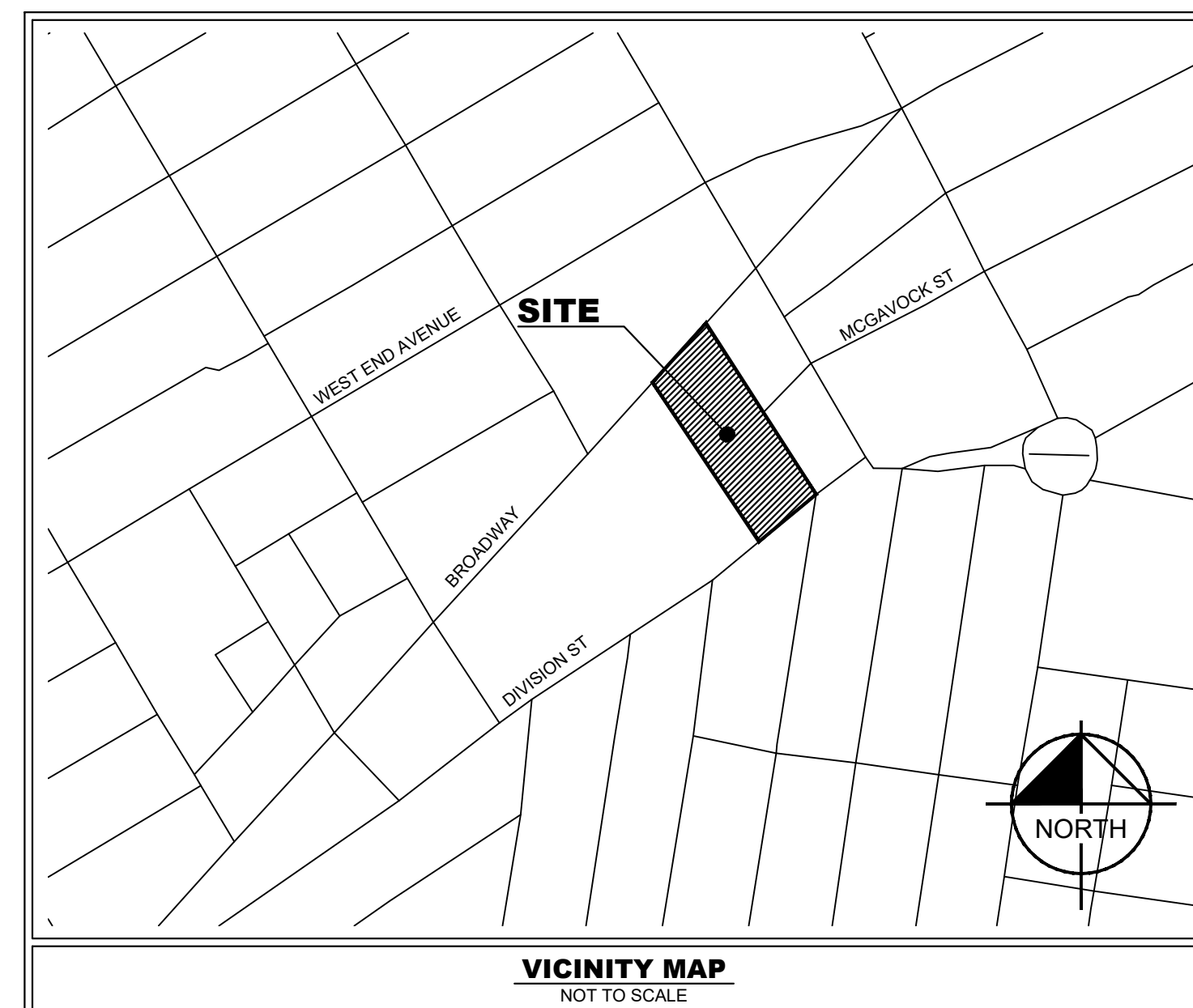
SITE DATA TABLE		
SITE ADDRESS	1711 BROADWAY NASHVILLE, TN, 37203	
TAX MAP	MAP 092-16 PARCEL 206	
OVERALL AREA	EXISTING	PROPOSED
SITE AREA	1.31 AC	1.26 AC
DISTURBED AREA	N/A	1.39 AC
DEDICATED R.O.W. AREA	N/A	0.05 AC
IMPERVIOUS AREA	1.009 AC	1.26 AC
PERVIOUS AREA	0.305 AC	0.00 AC
BUILDING DATA	REQUIRED	PROVIDED
USE	MULTIFAMILY RESIDENTIAL: RETAIL	
RESIDENTIAL	404,429 SF	
RESIDENTIAL F.A.R.	7.09	
RETAIL (MAX. 6,500 SF)	6,243 SF	
RETAIL F.A.R.	0.11	
MAX. BUILDING HEIGHT	20 STORIES	
ISR	1.0	
UNIT DENSITY	302.3 DU/A	
TOTAL UNITS (MAX. 400)	396 UNITS	
1-BR	272 UNITS	
2-BR	115 UNITS	
3-BR	9 UNITS	
BUILDING SETBACKS	REQUIRED	PROVIDED
FRONT: NORTH	0 FT	0 FT
REAR: SOUTH	0 FT	7 FT
SIDE: EAST	0 FT	17 FT
SIDE: WEST	0 FT	0 FT
PARKING DATA		
UZO TOTAL REQUIRED SPACES	426*	
UZO REQUIRED RESIDENTIAL SPACES	412*	
UZO REQUIRED RETAIL SPACES	14*	
PROVIDED SPACES	443	
PROVIDED RESIDENTIAL SPACES	423	
PROVIDED RETAIL SPACES	20	

NOTE	
*REQUIRED PARKING SHOWN REFLECTS THE 10% TRANSIT PARKING REDUCTION.	

EXISTING PROPERTY SUMMARY	
PROPERTY ADDRESS:	1711 BROADWAY NASHVILLE, TN
MAP AND PARCEL ID:	MAP 092-16, PARCEL 206
U.S. FEMA FIRM PANEL:	47037C0243H DATED 4/5/2017
EXISTING ZONING:	MU-A
EXISTING LAND USE:	RETAIL

METRO PERMITTING REFERENCE	
BUILDING PERMIT:	<b>TBD</b>
GRADING PERMIT:	<b>SWGR TBD</b>
WATER PERMIT:	<b>TBD</b>
SEWER PERMIT:	<b>TBD</b>

GENERAL DEVELOPMENT NOTES	
1.	ALL DEVELOPMENT WITHIN THE BOUNDARIES OF THIS PLAN MEETS THE REQUIREMENTS OF ANSII 117.1, 2009 EDITION AND THE FAIR HOUSING ACT.  U.S. Justice Department: <a href="http://www.justice.gov/crt/housing/fairhousing/about_fairhousingact.htm">http://www.justice.gov/crt/housing/fairhousing/about_fairhousingact.htm</a>
2.	THE SUBJECT PROPERTY IS LOCATED IN AREAS DESIGNATED AS "ZONE X" (AREAS OF MINIMAL FLOOD HAZARD) AS NOTED ON THE CURRENT FEMA FIRM COMMUNITY PANEL 47037C0243H.
3.	ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78/840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
4.	METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES IN THIS SITE.



**COUNCIL DISTRICT 19**  
**COUNCIL MEMBER FREDDIE O'CONNELL**  
**CITY OF NASHVILLE**  
**DAVIDSON COUNTY, TN**

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C6-00	UTILITY PLAN - OVERALL
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27	BACK COVER

### PROJECT DESIGN TEAM

#### ARCHITECT

NORR  
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CHICAGO, IL 60654  
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#### CIVIL ENGINEER

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#### GEOTECH / ENVIRONMENTAL

### PERMITTING / UTILITY CONTACTS

#### BUILDING DEPT.

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METRO WATER SERVICES  
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#### SANITARY SEWER

METRO WATER SERVICES  
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#### ZONING

METRO NASHVILLE ZONING  
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#### FIRE CHIEF

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#### URBAN FORESTER

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#### TDOT

TENNESSEE DEPARTMENT OF  
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#### NATURAL GAS

PIEDMONT NATURAL GAS  
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#### TELEPHONE

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#### OWNER / DEVELOPER

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CONTACT: GABRIEL LEAHU

#### PLANS PREPARED BY

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PLAN REVISIONS		
REVISION NO.	DATE	REMARKS
FILE NUMBER	SHEET NUMBER	TOTAL SHEETS
<b>118345001</b>	<b>C0-00</b>	<b>34</b>

**ENGINEER'S SEAL**

**DRAFT**

**PRELIMINARY**

**PLANS**

**FOR REVIEW ONLY**



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**CONTRACTOR RESPONSIBILITIES:**

- 1. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR:
A. THE CONTRACTOR SHALL VERIFY ALL PROPOSED AND EXISTING CONDITIONS INCLUDING UTILITIES (INVERTS, CONNECTIONS, MATERIALS, ETC.) AND DIMENSIONS WITHIN THE LIMITS OF WORK PRIOR TO THE START OF CONSTRUCTION.
B. REFER TO ARCHITECTURAL DRAWINGS FOR DETAILED BUILDING INFORMATION.
C. THE CONTRACTOR IS RESPONSIBLE FOR ALL NOTIFICATIONS AND LIAISONS WITH UTILITY COMPANIES DURING THE PROCESS OF LOCATING, RELOCATING, AND TYING INTO PUBLIC UTILITIES.
D. PRIOR TO COMMENCING LAND DISTURBANCE ACTIVITY, THE LIMITS OF LAND DISTURBANCE SHALL BE CLEARLY AND ACCURATELY DEMARCATED WITH STAKES, RIBBONS, OR OTHER APPROPRIATE MEANS. THE LOCATION AND EXTENT OF ALL AUTHORIZED LAND DISTURBANCE SHALL OCCUR INSIDE THE APPROVED LIMITS INDICATED ON THE APPROVED PLANS.
2. DURING CONSTRUCTION:
A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVIATIONS FROM THESE PLANS AND SPECIFICATIONS WITHOUT PRIOR WRITTEN CONSENT OF THE ENGINEER MAY CAUSE THE WORK TO BE UNACCEPTABLE.
B. THE CONTRACTOR SHALL USE MATERIALS AND EMPLOY CONSTRUCTION METHODS IN ORDER TO COMPLY WITH THE DRAWINGS AND SPECIFICATIONS. WHERE A CONFLICT OCCURS, THE STRICTEST DESIGN SHALL GOVERN. THE ENGINEER'S REVIEW OF SHOP DRAWINGS, PRODUCT DATA, ETC., DOES NOT RELIEVE THE CONTRACTOR FROM COMPLYING WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY SPECIFIC DEVIATIONS AND OBTAIN ENGINEER'S WRITTEN APPROVAL OF THE SPECIFIC DEVIATION.
C. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL APPLICABLE SAFETY CODES AND REGULATIONS DURING ALL PHASES OF CONSTRUCTION.
D. ALL CONSTRUCTION MUST CONFORM TO THE STANDARDS, SPECIFICATIONS, AND CODES OF THE GOVERNING MUNICIPALITIES.
E. CONSTRUCTION SHALL MEET ALL CURRENT STANDARDS SET FORTH IN THE AMERICANS WITH DISABILITIES ACT.
F. IF THE CONTRACTOR DAMAGES ANY EXISTING UTILITIES DURING CONSTRUCTION, HE SHALL, AT HIS OWN EXPENSE, REPLACE OR REPAIR THE UTILITIES TO ORIGINAL CONDITION AND QUALITY AS APPROVED BY THE OWNER AND REPRESENTATIVE OF THE APPROPRIATE UTILITY COMPANY.
G. SUFFICIENT BARRICADES, LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL METHODS IN ACCORDANCE WITH GOVERNING ORDINANCES MAY BE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC. SAID CONTROL DEVICES SHALL BE PER THE MANUAL OF TRAFFIC CONTROL DEVICES, M.U.T.C.D., CURRENT EDITION, AND SHALL BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION.
H. TRAFFIC CONTROLS AND OTHER WARNING DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY WORK ON CITY, COUNTY, OR TENNESSEE DEPARTMENT OF TRANSPORTATION ROADS. THEY SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION AND SHALL REMAIN IN PLACE UNTIL THE CONCLUSION OF ALL WORK.
I. ALL WARNING DEVICES SHALL BE EITHER TYPE I BARRICADES OR DRUMS WITH WARNING LIGHTS ON EVERY OTHER DEVICE. THEY SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION, AND METRO NASHVILLE STANDARDS FOR COLOR, SIZE, REFLECTIVITY, HEIGHT, AND PLACEMENT.
J. FIRE DEPARTMENT ACCESS SHALL BE MAINTAINED AT ALL TIMES.
K. CONTRACTOR SHALL SHORE AND BRACE ALL EARTH, FORMS, CONCRETE, STEEL, WOOD, AND MASONRY TO RESIST GRAVITY, EARTH, WIND, THERMAL, CONSTRUCTION, AND MISCELLANEOUS LOADS DURING CONSTRUCTION.
L. ON-SITE BURIAL OF DEBRIS IS PROHIBITED.
M. UNLESS OTHERWISE NOTED THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL FABRICATED MATERIALS TO THE ENGINEER. DESIGN DOCUMENTS SHALL NOT BE REPRODUCED AS SHOP DRAWINGS.
N. IN CASE OF UNFORESEEN CONSTRUCTION COMPLICATIONS OR DISCREPANCIES, THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE ENGINEER IN WRITING.
O. ALL REQUIRED TESTING REPORTS SHALL BE AVAILABLE AT THE JOB SITE.
P. AS-BUILT DRAWINGS OF ROADWAYS, STORM DRAINS, SANITARY SEWER AND WATER LINES, FIELD APPROVAL BY THE ENGINEER, AND ALL APPLICABLE BONDS ARE REQUIRED PRIOR TO FINAL ACCEPTANCE BY THE OWNER.
Q. CONTRACTOR SHALL MAINTAIN CONTINUOUS UTILITY SERVICE TO ALL EXISTING BUILDINGS THROUGHOUT CONSTRUCTION UNLESS APPROVAL FOR SERVICE INTERRUPTION IS OBTAINED FROM THE OWNERS IN ADVANCE.
R. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS TO ENSURE THAT THE NEW WORK SHALL FIT INTO THE EXISTING SITE IN THE MANNER INTENDED AND AS SHOWN ON THE DRAWINGS. SHOULD ANY CONDITIONS EXIST THAT ARE CONTRARY TO THOSE SHOWN ON THE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE PRIOR TO PERFORMING ANY WORK IN THE AREA INVOLVING DIFFERENCES. NOTIFICATION SHALL BE IN THE FORM OF A DRAWING OR SKETCH INDICATING FIELD MEASUREMENTS AND NOTES RELATING TO THE AREA.
S. ANY FOREIGN ITEM FOUND DURING CONSTRUCTION IS THE PROPERTY OF THE OWNER. THIS INCLUDES, BUT IS NOT LIMITED TO, PRECIOUS METALS, COINS, PAPER CURRENCY, ARTIFACTS AND ANTIQUITIES.
T. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE DONE TO THE PREMISES OR ADJACENT PREMISES, OR INJURIES TO THE PUBLIC DURING THE CONSTRUCTION OF THE WORK, WHETHER CAUSED BY HIMSELF, HIS SUBCONTRACTORS, OR THE CARELESSNESS OF ANY OF HIS EMPLOYEES.
U. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN ALL NECESSARY TEMPORARY WORKS FOR THE PROTECTION OF THE WORK AND THE PUBLIC, INCLUDING BARRICADES, WARNING SIGNS, LIGHTS, ETC.
V. THE CONTRACTOR ACKNOWLEDGES & AGREES THAT THE WORK IS ENTIRELY AT HIS RISK UNTIL SITE IS ACCEPTED, AND HE WILL BE HELD RESPONSIBLE FOR ITS SAFETY BY THE OWNER. THE CONTRACTOR WILL INDEMNIFY THE OWNER & OWNER'S REPRESENTATIVE FROM LIABILITY AT THE SITE THROUGHOUT THE CONSTRUCTION PROCESS.
W. THE CONTRACTOR SHALL GIVE ALL NECESSARY NOTICES AND OBTAIN ALL PERMITS AND PAY ALL LEGAL FEES. HE SHALL ALSO COMPLY WITH ALL CITY, COUNTY AND STATE BUILDING LAWS, ORDINANCES OR REGULATIONS RELATING TO BUILDING SIDEWALKS, STREETS, BLASTING, PUBLIC INFRASTRUCTURE, STORMWATER REGULATIONS, ETC.
X. THE CONTRACTOR IS TO CHECK AND VERIFY ALL MEASUREMENTS, LEVELS, ETC. BEFORE ORDERING MATERIALS AND PROCEEDING WITH THE WORK, AND IS TO BE RESPONSIBLE FOR THE SAME.
Y. REFERENCE POINTS AND HUBS DURING THE CONSTRUCTION OF HIS WORK, AND SHALL BEAR THE COST OF REPLACING SAME.
Z. CARE SHALL BE TAKEN TO PROTECT ANY UTILITIES, TREES, ETC. WHICH ARE TO REMAIN AND NOT TO BE DISTURBED BY THE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGES TO SUCH PROPERTY.

**DEMOLITION INFORMATION:**

- 1. NOTIFICATIONS:
THE CONTRACTOR SHALL NOTIFY THE OWNER AND CITY INSPECTOR(S) 24 HOURS PRIOR TO ANY DEMOLITION OR CONSTRUCTION.
2. DISPOSAL GUIDELINES:
A. ONLY ITEMS SPECIFICALLY NOTED TO BE DEMOLISHED SHALL BE REMOVED FROM THE SITE.
B. REMOVE EXISTING PAVED AREAS AS SHOWN INCLUDING DRIVEWAYS, SIDEWALKS, PARKING AREAS, SERVICE AREAS, EQUIPMENT PADS, AND ALL MISCELLANEOUS PAVING.
C. ALL DEBRIS RESULTING FROM DEMOLITION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF LEGALLY BY THE CONTRACTOR IN ACCORDANCE WITH LOCAL STATE AND FEDERAL REGULATIONS. BACKFILL ALL TRENCHES AND EXCAVATIONS RESULTING FROM DEMOLITION.
D. ALL DEMOLISHED MATERIAL BECOMES THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE NOTED.
3. TREE PROTECTION GUIDELINES:
PROTECT ALL EXISTING TREES NOTED "TO REMAIN" AND ALL ITEMS TO BE TURNED OVER TO THE OWNER DURING DEMOLITION. TAKE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES. ANY EXISTING ITEMS TO BE TURNED OVER TO THE OWNER WHICH ARE DAMAGED DURING DEMOLITION SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER. TREES WHICH ARE DAMAGED WILL BE REPLACED OR REIMBURSED AT A RATE TO BE DETERMINED BY THE OWNER.
4. UTILITIES:
A. PRIOR TO REMOVING OR ABANDONING ANY UTILITY THE CONTRACTOR SHALL VERIFY THAT NO UPSTREAM SERVICE WILL BE TERMINATED. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY TERMINATION NOT SHOWN ON THE PLANS.
B. ALL ABANDONED WATER LINES, STORM SEWER PIPE, SANITARY SEWER PIPES, GAS LINES, OR ANY OTHER ABANDONED UNDERGROUND UTILITY SHALL BE ABANDONED IN PLACE UNLESS NOTED OTHERWISE.

**SITE INFORMATION:**

- 1. THE FOLLOWING ARE APPLICABLE TO ALL CIVIL DOCUMENTS:
A. WHERE A DETAIL, SECTION, TYPICAL SECTION, OR A NOTE IS SHOWN FOR ONE CONDITION, IT SHALL APPLY FOR ALL LIKE OR SIMILAR CONDITIONS, UNLESS OTHERWISE NOTED ON THE PLANS.
B. EXISTING AND PROPOSED CONTOURS ARE AT ONE (1) FOOT INTERVALS.
C. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
D. ALL PIPE LENGTHS SPECIFIED IN THESE PLANS ARE THE HORIZONTAL DISTANCE AND ARE SHOWN FOR REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE ACTUAL LENGTHS BASED ON PROPOSED PIPE SLOPE.
E. PIPE LENGTHS IN PLANS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE UNLESS OTHERWISE NOTED.

**EROSION AND SEDIMENT CONTROL INFORMATION:**

- 1. COMPREHENSIVE:
A. THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO OR CONCURRENT WITH LAND-DISTURBING ACTIVITIES.
B. PROVISIONS TO PREVENT EROSION OF SOIL FROM THE SITE SHALL BE AT A MINIMUM IN CONFORMANCE WITH THE REQUIREMENTS OF THE TENNESSEE EROSION AND SEDIMENT CONTROL HANDBOOKS. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
C. FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB SITE UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE STANDARDS SPECIFIED IN THE TENNESSEE EROSION AND SEDIMENT CONTROL HANDBOOKS, CURRENT EDITION.
D. EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
E. EROSION CONTROL DEVICES SHALL BE INSTALLED PRIOR TO LAND DISTURBANCE. THE LOCATION OF SOME OF THE EROSION CONTROL DEVICES MAY HAVE TO BE ALTERED FROM THAT SHOWN ON THE APPROVED PLANS IF DRAINAGE PATTERNS DURING CONSTRUCTION ARE DIFFERENT FROM THE FINAL PROPOSED DRAINAGE PATTERNS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ACCOMPLISH EROSION CONTROL FOR ALL DRAINAGE PATTERNS CREATED AT VARIOUS STAGES DURING CONSTRUCTION. ANY DIFFICULTY IN CONTROLLING EROSION DURING ANY PHASE OF CONSTRUCTION SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
F. THE CONSTRUCTION OF THE SITE WILL COMMENCE WITH THE INSTALLATION OF EROSION CONTROL MEASURES SUFFICIENT TO CONTROL SEDIMENT DEPOSITS AND EROSION. ALL SEDIMENT CONTROL WILL BE MAINTAINED UNTIL ALL UPSTREAM GROUND WITHIN THE CONSTRUCTION AREA HAS BEEN COMPLETELY STABILIZED WITH PERMANENT VEGETATION AND ALL ROADS/DRIVEWAYS HAVE BEEN PAVED.
G. CONSTRUCTION EXITS SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE AND SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH STONE AS CONDITIONS DEMAND, REPAIR, AND/OR CLEANOUT OF ANY STRUCTURES USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OFF SITE ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. ACCESS POINTS PROTECTED WITH A CONSTRUCTION EXIT SHALL BE OTHERWISE BARRICADED UNTIL THE SITE IS STABILIZED.

**ADA COMPLIANCE:**

- A. CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
B. PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
C. ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
D. ANY COMPONENTS OF THE PROJECT SERVING MULTIFAMILY DWELLINGS IN BUILDINGS THAT HAVE 4 OR MORE UNITS PER DWELLING SHALL ALSO CONFORM TO THE FAIR HOUSING ACT (FHA), AND COMPLY WITH THE FAIR HOUSING ACT DESIGN MANUAL BY THE US DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT.
E. BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
F. CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.

**CONCRETE INFORMATION (SITE WORK ONLY):**

- 1. PRODUCT CRITERIA:
A. UNLESS OTHERWISE NOTED CEMENT SHALL BE TYPE I OR III CONFORMING TO ASTM C150. AGGREGATES SHALL BE NORMAL WEIGHT CONFORMING TO ASTM C33.
B. CONCRETE SHALL CONFORM TO ACI BUILDING CODE (318-89). UNLESS NOTED CONCRETE SHALL BE NORMAL WEIGHT AND HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3,500 P.S.I.
C. SLUMP SHALL BE 3" TO 5" FOR REGULAR MIX. LARGER SLUMP SHALL BE PERMITTED WITH WATER REDUCING ADMIXTURES AND WRITTEN CONSENT OF THE ENGINEER.
D. THE RELATIONSHIP BETWEEN MAXIMUM AGGREGATE SIZE TO MINIMUM AMOUNT OF CEMENT IN CONCRETE PAVEMENT (LB. PER C.Y. OR MIX) SHALL BE AS FOLLOWS: 1" - 520, 3/4" - 540, 1/2" - 590, 3/8" - 610.
2. CURING CRITERIA:
A. CONCRETE CURING SHALL COMPLY WITH ACI 308. CURING PROCESS SHALL START IMMEDIATELY FOLLOWING INITIAL SET. CURING SHALL BE BY CURING COMPOUND.
B. CONCRETE EXPOSED TO THE WEATHER SHALL BE AIR-ENTRAINED IN ACCORDANCE WITH ACI 318-89 TABLE 4.1.1. NORMAL WEIGHT CONCRETE SLABS SHALL HAVE AIR CONTENT IN ACCORDANCE WITH ACI 302.1R-89 TABLE 5.2.7A.
C. HOT WEATHER CONCRETING SHALL COMPLY WITH ACI 305. NO CONCRETE ABOVE 90 DEGREES FAHRENHEIT SHALL BE POURED. LOWER CONCRETE TEMPERATURE BY COOLING WATER AND AGGREGATE. FORMS, STEEL, AND SUBGRADE SHALL BE SPRINKLED WITH COLD WATER. AFTER FINISHING CONCRETE USE LIGHT FOG SPRAY UNTIL CURING COMPOUND IS USED.
D. COLD WEATHER CONCRETING SHALL COMPLY WITH ACI 306. SPECIAL MATERIAL PROCEDURES SHALL BE PROVIDED DURING PLACING AND CURING OF CONCRETE BELOW 40 DEGREES FAHRENHEIT.
E. CURING, HOT, AND COLD WEATHER CONCRETING PROCEDURES ARE ONLY GIVEN AS A GUIDE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PREVENT CONCRETE DAMAGE AND CRACKS. DAMAGED OR CRACKED CONCRETE WILL NOT BE ACCEPTED.

**REINFORCING STEEL INFORMATION (SITE WORK ONLY):**

- 1. PRODUCT CRITERIA:
A. REINFORCING STEEL SHALL CONFORM TO ASTM A-615, GRADE 60 AND GRADE 40 FOR #3 AND SMALLER BARS. MINIMUM LAP 48" DIAMETER.
B. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A-185, MINIMUM LAP SHALL BE 8".
C. REINFORCING THAT IS WELDED SHALL BE WELDABLE TYPE AND CONFORM TO ASTM A-706.

**PAVEMENT INFORMATION:**

- 1. PAVEMENT:
A. ALL MATERIALS, EQUIPMENT, METHODS OF CONSTRUCTION, AND WORKMANSHIP SHALL CONFORM TO THE TENNESSEE DEPARTMENT OF TRANSPORTATION, TDOT, STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
B. SEE PAVEMENT DETAILS ON CONSTRUCTION DOCUMENTS FOR SPECIFIC DESIGN INFORMATION AND REQUIREMENTS.
C. ALL CURB AND GUTTER TO BE 24" AND CONSTRUCTED OF 3000 P.S.I. CONCRETE UNLESS OTHERWISE NOTED.
2. SIGNING AND STRIPING:
A. SIGNING AND STRIPING TO BE PROVIDED BY THE CONTRACTOR ACCORDING TO THE DRAWINGS AND SPECIFICATIONS.
B. ALL PAVEMENT MARKINGS SHALL CONFORM TO CURRENT MUTCD STANDARDS. ALL PAVEMENT MARKINGS ON PRIVATE PROPERTY SHALL BE PAINT, UNLESS NOTED OTHERWISE. ALL PAVEMENT MARKINGS ON PUBLIC RIGHT-OF-WAY SHALL BE THERMOPLASTIC, UNLESS NOTED OTHERWISE.

**STORM SEWER NOTES:**

- 1. REINFORCED CONCRETE PIPE SHALL BE RCP CLASS III UNLESS OTHERWISE NOTED WITH BELL-AND-SPIGOT AND GASKETED JOINTS WITH ASTM C 443 RUBBER GASKETS.
2. FILL HEIGHTS OVER 13' REQUIRE CLASS IV RCP STORM PIPE.
3. STORM INLETS SHALL BE PRECAST IN ACCORDANCE WITH METRO NASHVILLE PUBLIC WORKS SPECIFICATIONS WHICH MEET OR EXCEED ASTM C 478.
4. ALL MANHOLE FRAMES AND COVERS ARE TO BE PER METRO NASHVILLE PUBLIC WORKS STANDARD DETAIL FOR DIMENSIONS AND MATERIALS AND AS BELOW IF NOT OTHERWISE INDICATED. FERROUS; 24-INCH (610-MM) ID BY 7- TO 9-INCH (175- TO 225-MM) RISER WITH 4-INCH- (102-MM-) MINIMUM WIDTH FLANGE AND 26-INCH- (660-MM-) DIAMETER COVER. INCLUDE INDENTED TOP DESIGN WITH LETTERING CAST INTO COVER, USING WORDING EQUIVALENT TO "STORM SEWER."
5. MATERIAL: GRAY IRON ASTM A48 CLASS 30 UNLESS OTHERWISE INDICATED.
6. ALL HDPE PIPE SHALL BE CORRUGATED HIGH DENSITY POLYETHYLENE SMOOTH INTERIOR PIPE. HDPE PIPE SHALL CONFORM TO ASTM D3350 WITH SOIL TIGHT JOINTS.
7. ALL HDPE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND AASHTO SECTION 30.
8. CONTRACTOR TO PROVIDE AND INSTALL MANUFACTURER RECOMMENDED FITTINGS ON RCP CONNECTIONS TO HDPE STRUCTURES.
9. REFER TO PIPE CHART FOR CASTING TYPES. INSTALL REDUCERS AS NECESSARY PER MANUFACTURER'S SPECIFICATIONS TO ACCOMMODATE LARGER INLET SIZES.
10. FOLLOW CONSTRUCTION PLANS AND MANUFACTURER DETAILS, SPECIFICATIONS, AND INSTALLATION INSTRUCTION AS INCLUDED WITHIN THE PLANS AND PROVIDED BY MANUFACTURER FOR THE INSTALLATION OF WATER QUALITY AND DETENTION SYSTEMS.
11. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL STORM SEWER PIPE, STRUCTURES, WATER QUALITY STRUCTURES, AND DETENTION STRUCTURES FOR ENGINEER AND OWNER APPROVAL PRIOR TO ORDERING MATERIALS.

**CLEARING AND GRUBBING:**

- A. DO NOT EXCEED CLEARING AND GRUBBING LIMITS OF CONSTRUCTION LINES INDICATED ON THE PLANS.
B. ALL AREAS OUTSIDE THE LIMITS OF CONSTRUCTION LINE SHALL NOT BE CROSSED BY HEAVY EQUIPMENT OR USED FOR STORING HEAVY EQUIPMENT OR MATERIALS.
C. NO EQUIPMENT SHALL BE STORED UNDER THE DRIP LINE OF TREES TO REMAIN.
D. DO NOT FALL ANY TREES OR PUSH PILLS OF DEBRIS AGAINST ANY TREES TO REMAIN.
E. REMOVE ALL STUMPS, ROCKS, ASPHALT & CONCRETE DEBRIS, ETC. AND DISPOSE OFF SITE IN ACCORDANCE WITH LOCAL, STATE & FEDERAL REGULATIONS.
F. CONTACT ALL UTILITY AUTHORITIES WHO HAVE LINES WITHIN THE CLEARING AND GRUBBING LIMITS BEFORE STARTING WORK
G. ALL CLEARING SHALL BE LIMITED TO AREAS TO BE GRADED WITHIN 15 CALENDAR DAYS.
E. ALL EROSION CONTROL SEDIMENT BARRIERS, SILT FENCES, AND TREE PROTECTION DEVICES SHALL BE INSTALLED PRIOR TO STARTING CLEARING AND GRUBBING.
F. AFTER STAKING IS COMPLETED, TREES WITHIN GRADING LIMITS TO BE SAVED WILL BE IDENTIFIED BY THE OWNER'S REPRESENTATIVE. FIELD CHANGES TO GRADING PLANS SHALL BE MADE FOR SMOOTH TRANSITION OF GRADES AROUND ALL TREES WHICH REQUIRE TREE WELLS WITHIN THE GRADING LIMITS.
G. ALL CLEARING SHALL BE LIMITED TO AREAS TO BE GRADED WITHIN 15 CALENDAR DAYS.

**GRADING NOTES:**

- A. TOPSOIL SHALL BE STORED ON SITE IN LOCATIONS APPROVED BY THE OWNER'S REPRESENTATIVE. DRAINAGE SHALL ROUT AROUND THESE TOPSOIL STOCKPILES FOR THE DURATION OF THE GRADING OPERATIONS. EROSION CONTROL MEASURES SHALL PREVENT THE LOSS OF TOPSOIL MATERIAL.
B. UNSUITABLE SOILS SHALL BE UNIFORMLY SPREAD ACROSS NON-STRUCTURAL FILL AREAS AND COVERED WITH TOPSOIL AND SEEDED.
C. FILL AREA SHALL BE PROOF-ROLLED WITH RUBBER-TIRED EQUIPMENT WITH A MINIMUM WEIGHT OF FIFTEEN TONS PRIOR TO BEGINNING FILL OPERATION. AREAS WHICH ARE SOFT OR UNSTABLE SHALL BE UNDERCUT UNTIL STABLE SOILS ARE FOUND. RE-COMPACTION OF THESE SOILS SHALL BE TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698 (STANDARD PROCTOR).
D. CUT AREA SHALL BE PROOF-ROLLED AFTER FINAL SUBGRADE IS ACHIEVED IN THE SAME MANNER AS FILLED AREAS. SOFT OR UNSTABLE SOILS SHALL BE SCARIFIED TO A DEPTH OF 12" AND RE-COMPACTED TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698 (STANDARD PROCTOR).
E. ALL GRADING SHALL BE COMPLETED TO THE LEVEL INDICATED BY THE SCOPE OF WORK LISTED IN THE BID DOCUMENTS.
F. ELEVATIONS SHOWN ON THE PLANS IS THE FINISH GRADE ELEVATION.
G. GRADING SHALL BE SEQUENCED SO THAT BASE STONE IS PLACED WITHIN 10 CALENDAR DAYS PF ACHIEVING OPTIMUM SUBGRADE COMPACTION.
H. SOILS TESTING LABORATORY/ SOILS ENGINEER, CONTRACTOR WILL EMPLOY A QUALIFIED SOILS TESTING LABORATORY/ ENGINEER TO OBSERVE THIS WORK AND MAKE TESTS AS REQUIRED.
I. HAVE EARTH BORROW FILL, AGGREGATE, AND TOPSOIL, AND STRUCTURAL FILL TESTED AND APPROVED BY DESIGNATED TESTING LABORATORY BEFORE MOVING IT TO THE JOB SITE.
J. CONTRACTOR SHALL OBSERVE PROOF-ROLLING OF AREAS WHERE BUILDING AND PAVING WILL BE LOCATED TO DETERMINE ADEQUACY OF SOIL COMPACTION AND IN-PLACE SOILS. OTHER AREAS WILL BE INSPECTED BY SOILS ENGINEER TO DETERMINE ADEQUACY IN THOSE AREAS. IF SOILS ARE NOT ADEQUATE TO BEAR WEIGHTS THAT WILL BE IMPOSED, TESTING LABORATORY WILL OBSERVE AND REPORT CORRECTIVE ACTION TAKEN.
K. TEST IN-PLACE SOIL AND FILLED AND COMPACTED AREAS. IF THESE ARE NOT ADEQUATE TO BEAR WEIGHTS IMPOSED, TESTING LABORATORY WILL ADVISE THE OWNER'S REPRESENTATIVE OF THEIR RECOMMENDATIONS. HE WILL DIRECT ANY CORRECTIVE MEASURES THAT ARE NECESSARY.
L. SOILS COMPACTION TESTING OF IN-PLACE AND FILLED AND COMPACTED AREAS WILL BE PERFORMED BY TESTING LABORATORY IN ACCORDANCE WITH THEIR REQUIREMENTS.
M. THE SOILS ENGINEER'S AND TESTING LABORATORY'S FEES WILL BE PAID BY THE CONTRACTOR.
N. APPLICABLE SPECIFICATIONS FOR COMPACTED FILL: THE FOLLOWING CURRENT AMERICAN SOCIETY OF TESTING MATERIALS (ASTM) STANDARDS ARE HEREBY MADE PART OF THIS SPECIFICATION:
• D421-58, DRY PREPARATION OF SOIL SAMPLES FOR GRAIN-SIZE ANALYSIS AND DETERMINATION OF SOIL CONSTANTS.
• D422-63, STANDARD METHOD OF PARTICLE SIZE ANALYSIS OF SOILS.
• D1140-54, METHOD OF TEST FOR AMOUNT OF MATERIAL IN SOILS FINER THAN NO.200 SIEVE.
• D698, METHOD FOR LABORATORY COMPACTION CHARACTERISTICS OF SOIL USING STANDARD EFFORT
• D1557-78, STANDARD TEST METHODS FOR MOISTURE-DENSITY RELATIONS OF SOILS AND SOIL-AGGREGATE MIXTURES USING 10 LB. (4.54-KG) RAMMER AND 18-INCH (457 MM) DROP.
O. PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS AT ALL LOCATIONS UNLESS OTHERWISE NOTED.
P. CONTRACTOR SHALL REVIEW THE SITE SPECIFIC GEOTECHNICAL REPORT PRIOR TO COMMENCING WITH GRADING OPERATIONS. WHERE CONFLICTS BETWEEN THE GRADING NOTES AND GEOTECHNICAL REPORT EXIST, THE MORE STRINGENT REQUIREMENT SHALL APPLY.
Q. CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING WITH LOAD TICKETS, PHOTOGRAPHS, LOG BOOK, VIDEO RECORDING AND OTHER MEANS AS NECESSARY TO VERIFY THE INSTALLATION OF STORMWATER BEST MANAGEMENT PRACTICES REQUIRED BY THE LOCAL MUNICIPALITY AND JURISDICTION. AS-BUILT TOPOGRAPHY AND UTILITY PLANS HAVING BEEN PREPARED BY A QUALIFIED LAND SURVEYOR ARE REQUIRED TO BE SUBMITTED TO KIMLEY-HORN AT THE CONCLUSION OF THE PROJECT FOR VERIFICATION OF DESIGN INTENT. ANY MODIFICATIONS TO THE GRADING AND UTILITY SYSTEMS REQUIRED, NOT PREVIOUSLY APPROVED BY THE OWNER AND ENGINEER ARE THE RESPONSIBILITY OF THE CONTRACTOR.



1711 BROADWAY LG DEVELOPMENT GROUP NASHVILLE, TN

DRAFT PRELIMINARY PLANS FOR REVIEW ONLY

Table with columns for DATE, BY, and REVISIONS.

DESIGNED BY:
DRAWN BY:
CHECKED BY:
DATE: 5/17/2022
KIMLEY-HORN PROJECT NO. 118345001

GENERAL NOTES

SHEET NUMBER C0-01



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**SURVEY NOTES**

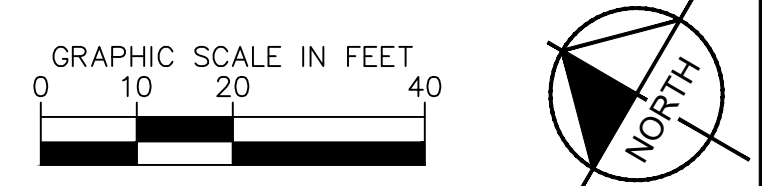
1. THE INFORMATION SHOWN ON THIS PLAN IS BASED UPON A COMBINATION OF DATA SOURCES INCLUDING AERIAL MAPS, METRO NASHVILLE GIS MAPS, AND METRO NASHVILLE PARCEL VIEWER.
2. THE INFORMATION PROVIDED SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION. KIMLEY-HORN AND ASSOCIATES SHALL NOT BE HELD RESPONSIBLE FOR THE ACCURACY AND COMPLETENESS OF THE BASE INFORMATION SHOWN.
3. NO PORTION OF THIS PROJECT IS LOCATED IN A FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FIRM MAPS NO. 47037C0243H, DATED APRIL 5, 2017.



NO.	DATE	BY	REVISIONS
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DESIGNED BY: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: 5/17/2022  
 KIMLEY-HORN PROJECT NO. 118345001

EXISTING CONDITIONS  
 SHEET NUMBER  
**C1-00**





OWNER CHART			
OWNER	TAX PARCEL NO.	PARCEL #	ADDRESS
U.S. BANK NATIONAL	09216020600	206	1711 BROADWAY, NASHVILLE, TN 37203

PRELIMINARY DEVELOPMENT SCHEDULE	
GRADING PERMIT / SITE WORK	MONTH 0
BUILDING PERMIT / BUILDING CONSTRUCTION START	MONTH 2
CONSTRUCTION COMPLETION / CO / PROJECT OPENING	MONTH 30

**LANDSCAPING NOTES**

PROPOSED LANDSCAPING WILL SATISFY ALL REQUIREMENTS OF CHAPTER 17.24 INCLUDING BUFFERING AND DUMPSTER SCREENING REQUIREMENTS.

- FINAL SP NOTE**
- THE FINAL SITE PLAN/BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.
  - LOADING ZONE LAYOUT AND CONFIGURATION SHALL BE FINALIZED AT FINAL SP. LAYOUT SHALL BE BASED ON THE EXPECTED SITE NEEDS AND SHALL ACCOMMODATE AN SU-30 DESIGN VEHICLE AT A MINIMUM.
  - BICYCLE PARKING SPACES SHALL BE FINALIZED AT FINAL SP. WHERE SUBSURFACE CONDITIONS MAKE BICYCLE PARKING DIFFICULT OR INFEASIBLE, THE ZONING ADMINISTRATOR BASED ON A RECOMMENDATION FROM THE PLANNING DEPARTMENT, MAY REDUCE OR WAIVE THE REQUIRED BICYCLE PARKING AND/OR MAY APPROVE DIFFERENT DESIGN AND LOCATION ARDS FOR REQUIRED BICYCLE PARKING AREAS.

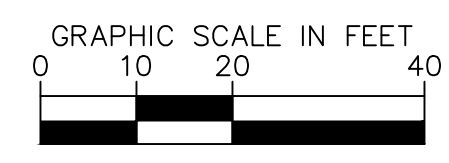
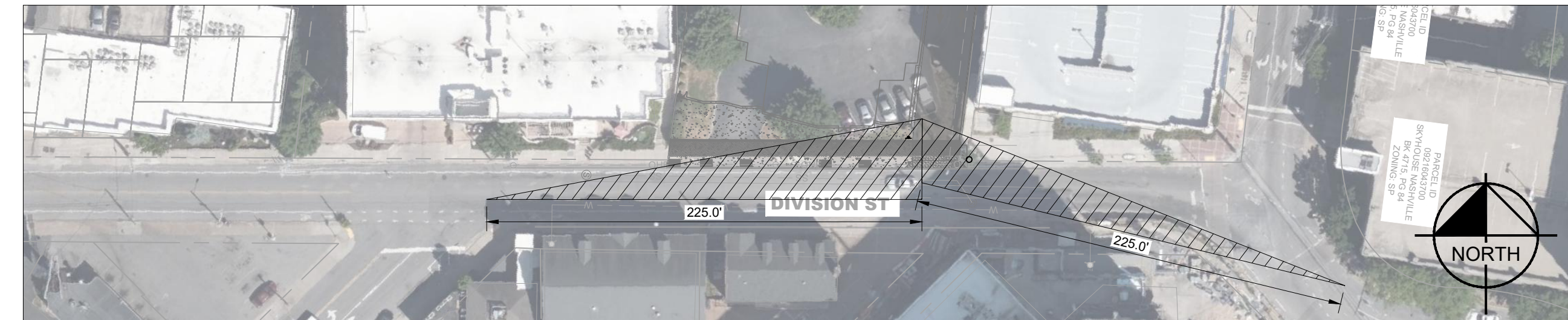
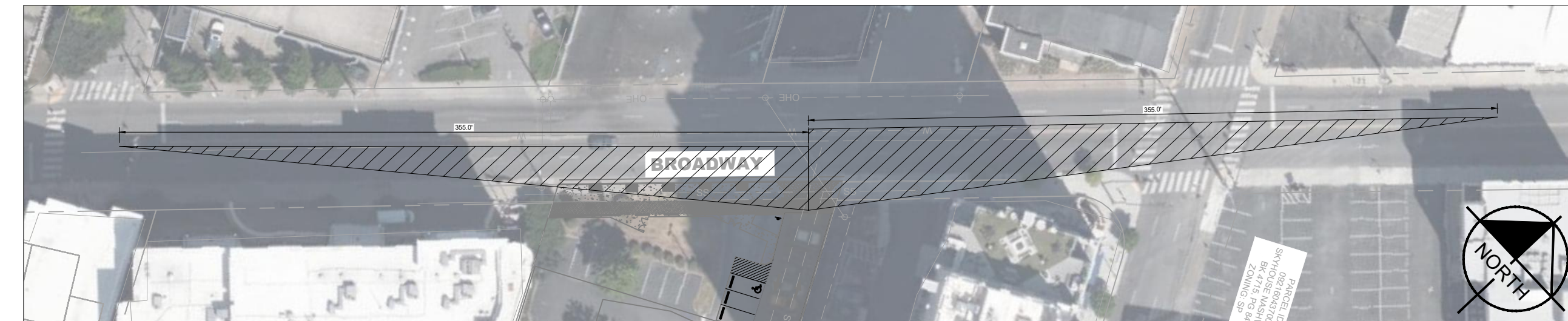
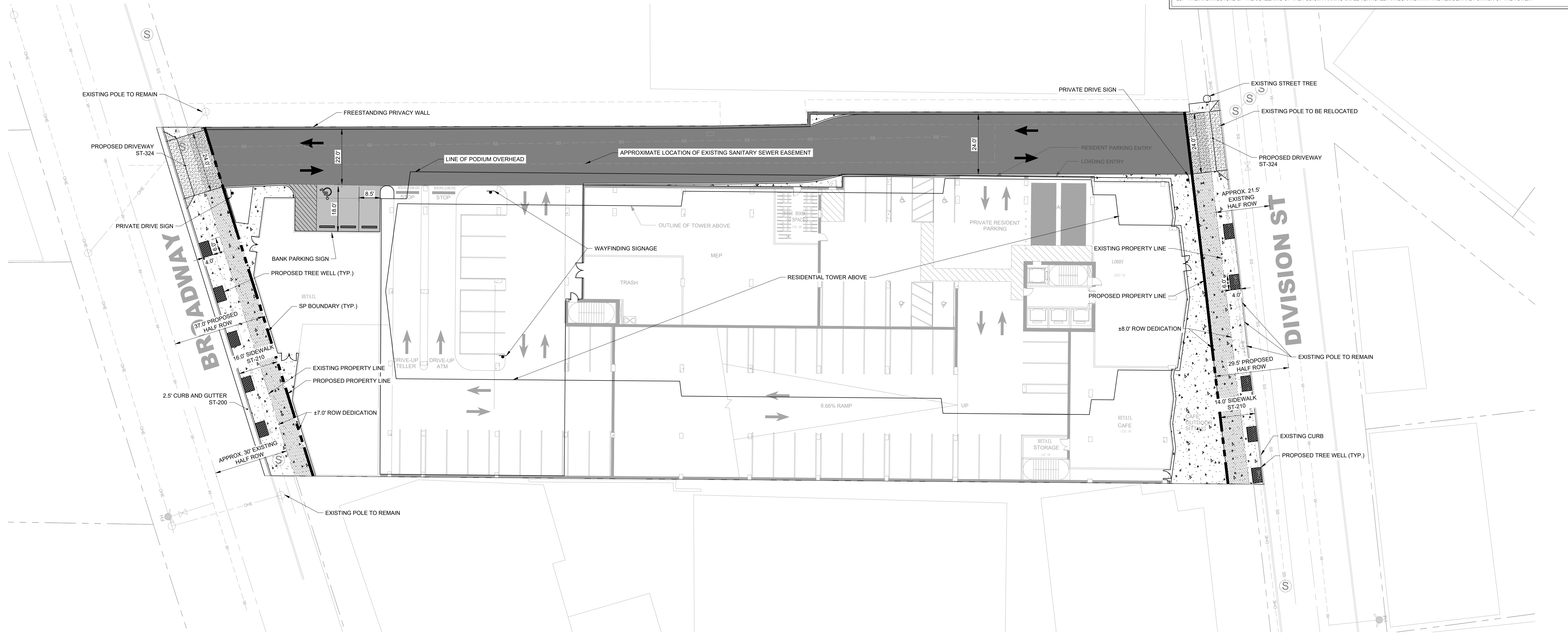
- NDOT NOTES**
- THE DEVELOPER'S FINAL CONSTRUCTION DRAWINGS SHALL COMPLY WITH THE DESIGN REGULATIONS ESTABLISHED BY THE NASHVILLE DEPARTMENT OF TRANSPORTATION, IN EFFECT AT THE TIME OF APPROVAL OF THE PRELIMINARY DEVELOPMENT PLAN OR FINAL DEVELOPMENT PLAN OR BUILDING PERMIT, AS APPLICABLE. FINAL DESIGN MAY VARY BASED ON FIELD CONDITIONS.
  - ALL ROW DEDICATIONS ARE TO BE RECORDED PRIOR TO ISSUANCE OF THE BUILDING PERMITS.
  - STORMWATER DISCHARGES ARE TO BE COORDINATED WITH NDOT AND METRO STORMWATER. POINT SOURCE DISCHARGE WILL NOT BE PERMITTED.

- SITE LAYOUT NOTES**
- INSTALL CONCRETE JOINTS WHERE SHOWN ON PLANS AND DETAILS. ALIGN ON WALLS, BUILDINGS, RADI, ETC. EVENLY SPACE BETWEEN ELEMENTS AS SHOWN. PROVIDE EXPANSION JOINTS BETWEEN CONCRETE PAVEMENT AND ALL VERTICAL ELEMENTS (WALLS, CURBS, ETC.)
  - LAYOUT ALL CURVES SMOOTHLY WITH NO ABRUPT CHANGES AT TANGENT POINTS.
  - ALL CURB RADI ARE 3 FT UNLESS OTHERWISE NOTED.
  - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
  - LAYOUT ALL ELEMENTS IN FIELD AND CONTACT OWNER'S REPRESENTATIVE FOR APPROVAL BEFORE BEGINNING ANY CONSTRUCTION.
  - CONTRACTOR TO TAKE ALL PRECAUTIONS TO FIND AND AVOID SITE UTILITIES. ALL UTILITIES ARE NOT SHOWN ON DRAWINGS. VERIFY LOCATIONS AND CONSIDER SUCH WHEN ESTIMATING.
  - ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
  - ALL PAVEMENT MARKINGS AND SIGNAGE SHALL BE INSTALLED PER THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
  - ALL SIDEWALK WITHIN THE RIGHT-OF-WAY SHALL BE CONSTRUCTED PER NDOT STANDARD DWG NO. ST-210.
  - SAW CUT LINES SHALL BE DONE IN A STRAIGHT NEAT LINE A MINIMUM OF 18" FROM THE EXISTING EDGE OF PAVEMENT.
  - ALL TREE GRATES SHALL BE ADA COMPLIANT AND H20 TRAFFIC RATED.
  - REFER TO SITE LANDSCAPE PLANS BY OTHERS FOR STREETSCAPE DETAILS.

- 1711 BROADWAY SP DEVELOPMENT STANDARDS**
- THE PURPOSE OF THIS PLAN IS TO REZONE TO PRELIMINARY SPECIFIC PLAN ZONING TO PERMIT UP TO 400 MULTI-FAMILY RESIDENTIAL UNITS AND 6,500 SQUARE FEET OF RETAIL.
  - ANY EXCAVATION, FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.
  - THIS PROPERTY DOES NOT LIE WITHIN A FLOOD HAZARD AS IDENTIFIED BY FEMA ON MAP 47037C0243H, DATED 04/05/2017.
  - ALL PUBLIC SIDEWALKS ARE TO BE CONSTRUCTED IN CONFORMANCE WITH NDOT SIDEWALK DESIGN STANDARDS.
  - WHEEL CHAIR ACCESSIBLE CURB RAMP: COMPLYING WITH APPLICABLE NDOT STANDARD, SHALL BE CONSTRUCTED AT STREET CROSSINGS.
  - THE REQUIRED FIRE FLOW SHALL BE DETERMINED BY THE METRO FIRE MARSHAL'S OFFICE, PRIOR TO THE ISSUANCE OF A BUILDING PERMIT.
  - SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANUAL, MINIMUM DRIVEWAY CULVERT IN METRO RIGHT OF WAY IS 15" CMP.
  - METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.
  - MINOR MODIFICATIONS TO THE PRELIMINARY SP MAY BE APPROVED BY THE PLANNING COMMISSION OR ITS DESIGNEE BASED UPON FINAL ARCHITECTURAL, ENGINEERING OR SITE DESIGN AND ACTUAL SITE CONDITIONS. ALL MODIFICATIONS SHALL BE CONSISTENT WITH THE PRINCIPLES AND FURTHER THE OBJECTIVES OF THE APPROVED PLAN. MODIFICATIONS SHALL NOT BE PERMITTED, EXCEPT THROUGH AN ORDINANCE APPROVED BY METRO COUNCIL, THAT INCREASE THE PERMITTED DENSITY OR FLOOR AREA. ADD USES NOT OTHERWISE PERMITTED, ELIMINATE THROUGH THIS ENACTING ORDINANCE, OR ADD VEHICULAR ACCESS POINTS NOT CURRENTLY PRESENT OR APPROVED.
  - IF A DEVELOPMENT STANDARD, NOT INCLUDING PERMITTED USES, IS ABSENT FROM THE SP PLAN AND/OR COUNCIL APPROVAL, THE PROPERTY SHALL BE SUBJECT TO THE STANDARDS, REGULATIONS AND REQUIREMENTS OF THE MUG-NS ZONING DISTRICT AS OF THE DATE OF THE APPLICABLE REQUEST OR APPLICATION. USES ARE LIMITED AS DESCRIBED IN THE COUNCIL ORDINANCE.
  - APPROVAL OF ANY SPECIFIC PLAN DOES NOT EXEMPT ANY PARCEL SHOWN ON THE PLAN OR ANY DEVELOPMENT WITHIN THE SP FROM COMPLIANCE WITH ALL PROVISIONS OF THE METRO ZONING CODE WITH RESPECT TO FLOODPLAIN, STEEP SLOPES, UNSTABLE SOILS, SINKHOLES, ROCK OUTCROPPINGS, STREAMS, SPRINGS, AND CRITICAL LOTS.
  - ALL SURFACE PARKING AREAS MUST MEET THE "PARKING AREA SCREENING AND LANDSCAPING" REQUIREMENTS SPECIFIED IN THE METRO ZONING CODE.
  - ACCORDING TO THE MUGS SOIL MAP, THE SOIL ON THE PROPERTIES IS MAURY-URBAN.
  - SITE SLOPES RANGE 0-7%. THERE ARE NO NATURAL SLOPES GREATER THAN 7%.
  - THERE ARE NO KNOWN EXISTING WETLANDS WITHIN THE SP BOUNDARY.
  - STORAGE SHALL MEET METRO DESIGN STANDARDS AND BE REVIEWED WITH THE BUILDING PLANS.
  - ALL DEVELOPMENTS WITHIN THE BOUNDARIES OF THIS PLAN SHALL BE BASED ON THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND THE FAIR HOUSING ACT, AS APPLICABLE.
  - THE REQUIREMENTS OF METRO FIRE MARSHAL'S OFFICE ADEQUATE WATER SUPPLY FOR FIRE PROTECTION MUST BE MET PRIOR TO THE ISSUANCE OF ANY BUILDING PERMITS.
  - ALL PROPOSED PUBLIC UTILITIES AND SERVICES SHALL BE INSTALLED UNDERGROUND.
  - THE OWNER/DEVELOPER AGREES TO MAINTAIN PRIVATE TRASH AND RECYCLING SERVICE FOR THE DEVELOPMENT AS LONG AS THE DEVELOPMENT REMAINS IN PLACE.
  - TWO RECORDING STUDIOS, A PODCAST STUDIO, AND A REHEARSAL ROOM THAT CAN BE RENTED BY THE PUBLIC WILL BE INCLUDED WITH THE DEVELOPMENT.
  - ALL FACADE MATERIALS, EXCLUSIVE OF CLEAR FENESTRATION, SHALL BE HIGH QUALITY AND SELECTED FROM THE FOLLOWING LIST: MASONRY, MASONRY PANELS, TEXTURED METAL, METAL PANELING, PRECAST CONCRETE, PRECAST CONCRETE PANEL, FIBER CEMENT PANEL, FIBER CEMENT SIDING, SPANDREL GLASS (ON UPPER STORIES ONLY) OR MATERIALS SUBSTANTIALLY SIMILAR IN FORM AND FUNCTION. THIS REQUIREMENT APPLIES TO ALL FACADES.
  - THE ARCHITECTURE OF THE SCREENING OF THE PODIUM PARKING SHALL VERTICALLY INTEGRATE WITH THE RESIDENTIAL PORTION OF THE TOWER.

**HATCH LEGEND**

[Hatch Pattern]	STANDARD DUTY CONCRETE SIDEWALK
[Hatch Pattern]	HEAVY DUTY CONCRETE PAVEMENT
[Hatch Pattern]	HEAVY DUTY ASPHALT PAVEMENT
[Hatch Pattern]	STANDARD ASPHALT PAVEMENT
[Hatch Pattern]	ROW DEDICATION



**DRAFT  
PRELIMINARY  
PLANS**  
FOR REVIEW ONLY

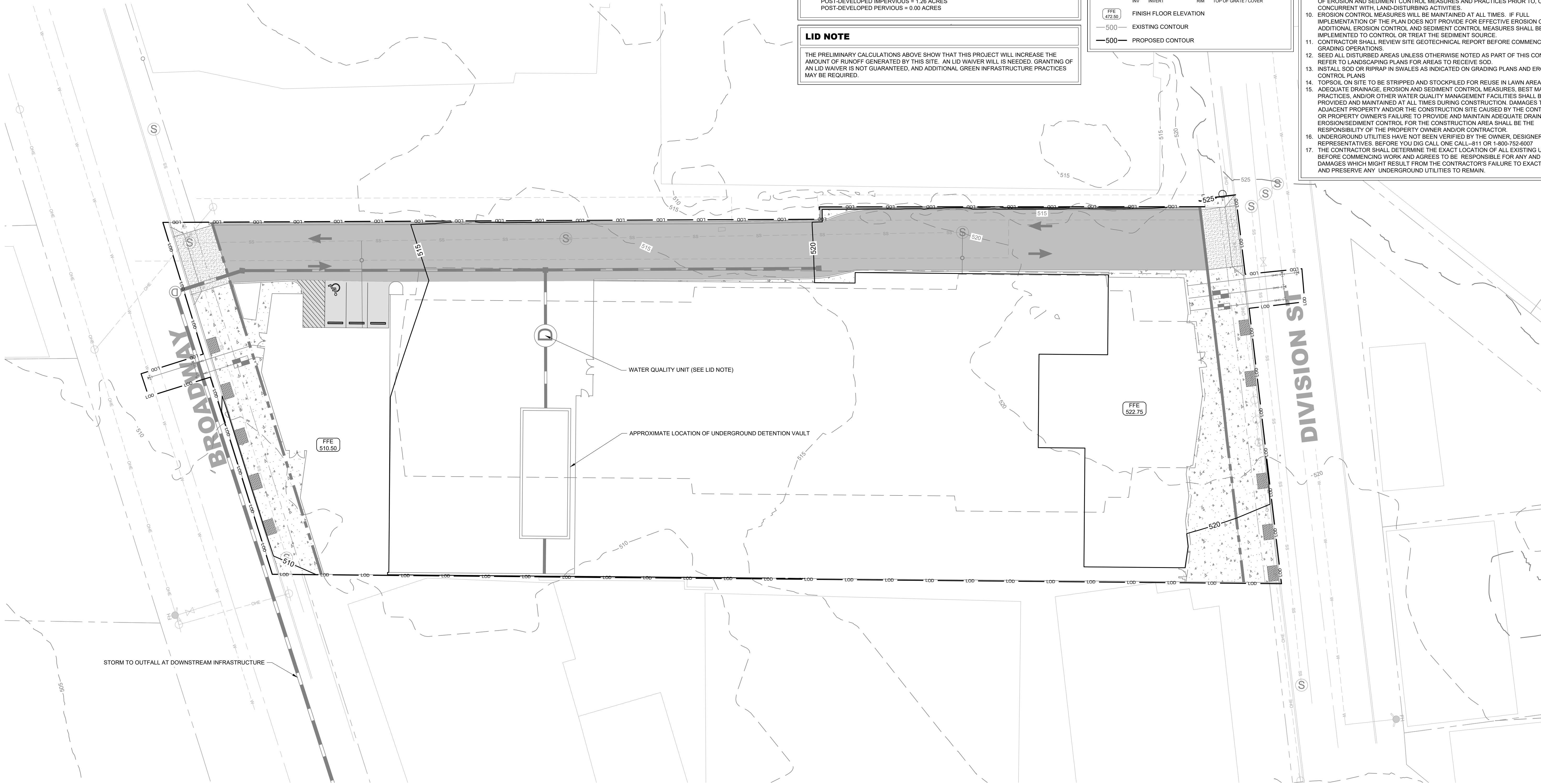
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DESIGNED BY: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: 5/17/2022  
KIMLEY-HORN PROJECT NO. 118345001

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**STORMWATER NOTES**

- THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL UNIT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF THE FINAL APPLICATION.
- THE SOILS TYPES FOR THIS SITE ARE MAURY-URBAN LAND COMPLEX WITH SLOPES RANGING FROM 2 TO 7%.
- METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES IN THIS SITE.
- ANY REQUIRED DRIVEWAY CULVERTS SHALL BE SIZED PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IS METRO ROW IS 15' RCP).
- WATER QUALITY AND QUANTITY REQUIREMENTS FOR THE SITE SHALL BE BASED ON THE CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL.
- IF REQUIRED, DETENTION SHALL TAKE PLACE IN AN UNDERGROUND DETENTION SYSTEM OR AN ABOVE GROUND VAULT. FINAL DESIGN AND DETAILS WILL BE INCLUDED WITH FINAL APPLICATION.
- THIS SITE WILL APPLY FOR AN LID WAIVER. FINAL DESIGN AND DETAILS WILL BE INCLUDED WITH FINAL APPLICATION.
- STORMWATER CONNECTIONS TO THE EXISTING SYSTEM SHALL BE TO EITHER STORM ONLY LINES OR TO COMBINED SEWER LINES WITH A MINIMUM DIAMETER OF 15".
- STORM SEWER SYSTEM ON THIS PLAN IS SHOWN SCHEMATICALLY. FINAL DESIGN WILL BE PROVIDED DURING THE FINAL SP PROCESS.

**PRE/POST CALCULATIONS**

**PRE-DEVELOPMENT**  
 TOTAL SITE AREA = 1.31 ACRES  
 PRE-DEVELOPED IMPERVIOUS = 1.009 ACRES  
 PRE-DEVELOPED PERVIOUS = 0.305 ACRES

**POST-DEVELOPMENT**  
 TOTAL SITE AREA = 1.26 ACRES  
 POST-DEVELOPED IMPERVIOUS = 1.26 ACRES  
 POST-DEVELOPED PERVIOUS = 0.00 ACRES

**LID NOTE**

THE PRELIMINARY CALCULATIONS ABOVE SHOW THAT THIS PROJECT WILL INCREASE THE AMOUNT OF RUNOFF GENERATED BY THIS SITE. AN LID WAIVER IS NEEDED. GRANTING OF AN LID WAIVER IS NOT GUARANTEED, AND ADDITIONAL GREEN INFRASTRUCTURE PRACTICES MAY BE REQUIRED.

**METRO STORMWATER NOTES**

**FEMA NOTE:**  
 THIS LOT DOES NOT LIE IN AN AREA DESIGNATED AS A SPECIAL FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP 47037C0243H, EFFECTIVE 4/5/2017.

**CONSTRUCTION SCHEDULE:**

- PRE-CONSTRUCTION MEETING
- INSTALLATION OF EROSION CONTROL MEASURES
- EROSION INSPECTION BY ERSC AND METRO
- ISSUANCE OF GRADING PERMIT
- CONSTRUCTION

CONSTRUCTION SHALL BE COMPLETED WITHIN 12 MONTHS OF THE GRADING PERMIT BEING ISSUED. IF CONSTRUCTION IS NOT COMPLETE IN THAT AMOUNT OF TIME, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO APPLY FOR AN EXTENSION OF THE GRADING PERMIT.

**DEC NOTICE OF COVERAGE NOTE:**  
 THIS PROJECT DOES NOT DISTURB MORE THAN 1 ACRE AND IS NOT REQUIRED TO APPLY FOR A NOTICE OF COVERAGE UNDER THE TENNESSEE GENERAL CONSTRUCTION PERMIT FROM TDEC.  
 THE TOTAL DISTURBED AREA IS ±1.39 ACRES.

**GRADING PLAN LEGEND**

- SPOT ELEVATION
  - HP HIGH POINT
  - TC TOP OF CURB
  - TW TOP OF WALL
  - INV INVERT
  - LP LOW POINT
  - BC BOTTOM OF CURB / GUTTER LINE
  - BW BOTTOM OF EXPOSED WALL
  - RM TOP OF GRADE COVER
- FINISH FLOOR ELEVATION
  - 500 EXISTING CONTOUR
  - 500 PROPOSED CONTOUR

**GRADING NOTES**

- CONTRACTOR RESPONSIBLE FOR VERIFYING LOCATION, SIZE, AND ELEVATIONS OF EXISTING UTILITIES AT CONNECTION POINTS PRIOR TO GRADING OR INSTALLATION OF ANY PROPOSED UTILITIES. CONTRACTOR TO IMMEDIATELY NOTIFY OWNERS REPRESENTATIVE IF DISCREPANCIES ARE FOUND.
- AREAS FOR CONSTRUCTION THAT REQUIRE DE-WATERING FOR EXCAVATION WILL BE DONE AT NO ADDITIONAL COST TO THE OWNER AND IN ACCORDANCE WITH SECTION 4.1.4 OF THE TENNESSEE GENERAL NPDES PERMIT FOR DISCHARGES OF STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
- ADDITIONAL EROSION CONTROL DEVICES TO BE USED AS REQUIRED BY LOCAL INSPECTOR.
- DISTURBED AREAS LEFT IDLE FOR FIVE DAYS, AND NOT TO FINAL GRADE, WILL BE ESTABLISHED TO TEMPORARY VEGETATION, MULCH, TEMPORARY VEGETATION OR PERMANENT VEGETATION SHALL BE COMPLETED ON ALL EXPOSED AREAS WITHIN 14 DAYS AFTER DISTURBANCE. ALL AREAS TO FINAL GRADE WILL BE ESTABLISHED TO PERMANENT VEGETATION UPON COMPLETION.
- WHEN HAND PLANTING, MULCH (HAY OR STRAW) SHOULD BE UNIFORMLY SPREAD OVER SEEDED AREA WITHIN 24 HOURS OF SEEDING. IF UNABLE TO ACCOMPLISH, MULCH SHALL BE USED AS A TEMPORARY COVER. CONCENTRATED FLOW AREAS AND ALL SLOPES STEEPER THAN 2.5:1 AND WITH A HEIGHT OF TEN FEET OR GREATER (DOES NOT APPLY TO RETAINING WALLS), AND CUTS AND FILLS WITHIN BUFFERS, SHALL BE STABILIZED WITH THE APPROPRIATE EROSION CONTROL MATTING OR BLANKETS.
- THE GRADING PERMIT MUST BE DISPLAYED ON SITE AT ALL TIMES DURING CONSTRUCTION AND IN PLAIN VIEW FROM A PUBLIC ROAD OR STREET.
- EROSION AND SEDIMENT CONTROL DEVICES MUST BE DISPLAYED AND INSPECTED PRIOR TO ANY GRADING ON SITE. THE CONTRACTOR MUST CALL FOR AN INSPECTION TO OBTAIN A PERMIT TO GRADE. PLEASE CALL WITH ENOUGH LEAD-TIME FOR AN INSPECTION TO MEET YOUR SCHEDULE.
- SEDIMENT/EROSION CONTROL DEVICES MUST BE INSPECTED ACCORDING TO LOCAL AND STATE REQUIREMENTS AND AS STIPULATED IN THE STORMWATER POLLUTION PREVENTION PLAN. EACH DEVICE IS TO BE MAINTAINED OR REPLACED IF SEDIMENT ACCUMULATION HAS REACHED ONE HALF THE CAPACITY OF THE DEVICE. ADDITIONAL DEVICES MAY BE NECESSARY AS THE PROJECT PROGRESSES AND NEW CHANNELS HAVE DEVELOPED.
- THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO, OR CONCURRENT WITH, LAND-DISTURBING ACTIVITIES.
- EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION CONTROL AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- CONTRACTOR SHALL REVIEW SITE GEOTECHNICAL REPORT BEFORE COMMENCING GRADING OPERATIONS.
- SEED ALL DISTURBED AREAS UNLESS OTHERWISE NOTED AS PART OF THIS CONTRACT. REFER TO LANDSCAPING PLANS FOR AREAS TO RECEIVE SOD.
- INSTALL SOD OR RIPRAP IN SWALES AS INDICATED ON GRADING PLANS AND EROSION CONTROL PLANS.
- TOPSOIL ON SITE TO BE STRIPPED AND STOCKPILED FOR REUSE IN LAWN AREAS.
- ADEQUATE DRAINAGE, EROSION AND SEDIMENT CONTROL MEASURES, BEST MANAGEMENT PRACTICES, AND/OR OTHER WATER QUALITY MANAGEMENT FACILITIES SHALL BE PROVIDED AND MAINTAINED AT ALL TIMES DURING CONSTRUCTION. DAMAGES TO ADJACENT PROPERTY AND/OR THE CONSTRUCTION SITE CAUSED BY THE CONTRACTOR'S OR PROPERTY OWNER'S FAILURE TO PROVIDE AND MAINTAIN ADEQUATE DRAINAGE AND EROSION/SEDIMENT CONTROL FOR THE CONSTRUCTION AREA SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER AND/OR CONTRACTOR.
- UNDERGROUND UTILITIES HAVE NOT BEEN VERIFIED BY THE OWNER, DESIGNER, OR THEIR REPRESENTATIVES. BEFORE YOU DIG CALL ONE CALL-811 OR 1-800-752-0077.
- THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT RESULT FROM THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY UNDERGROUND UTILITIES TO REMAIN.

**Kimley»Horn**

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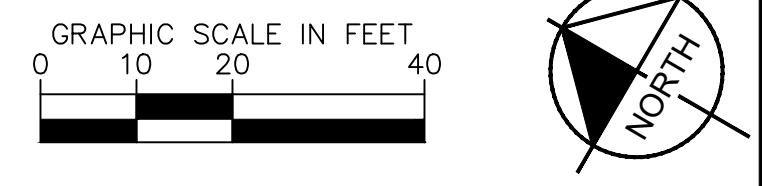
**1711 BROADWAY  
 LG DEVELOPMENT GROUP  
 NASHVILLE, TN**

**DRAFT  
 PRELIMINARY  
 PLANS  
 FOR REVIEW ONLY**

NO.	DATE	BY	REVISIONS
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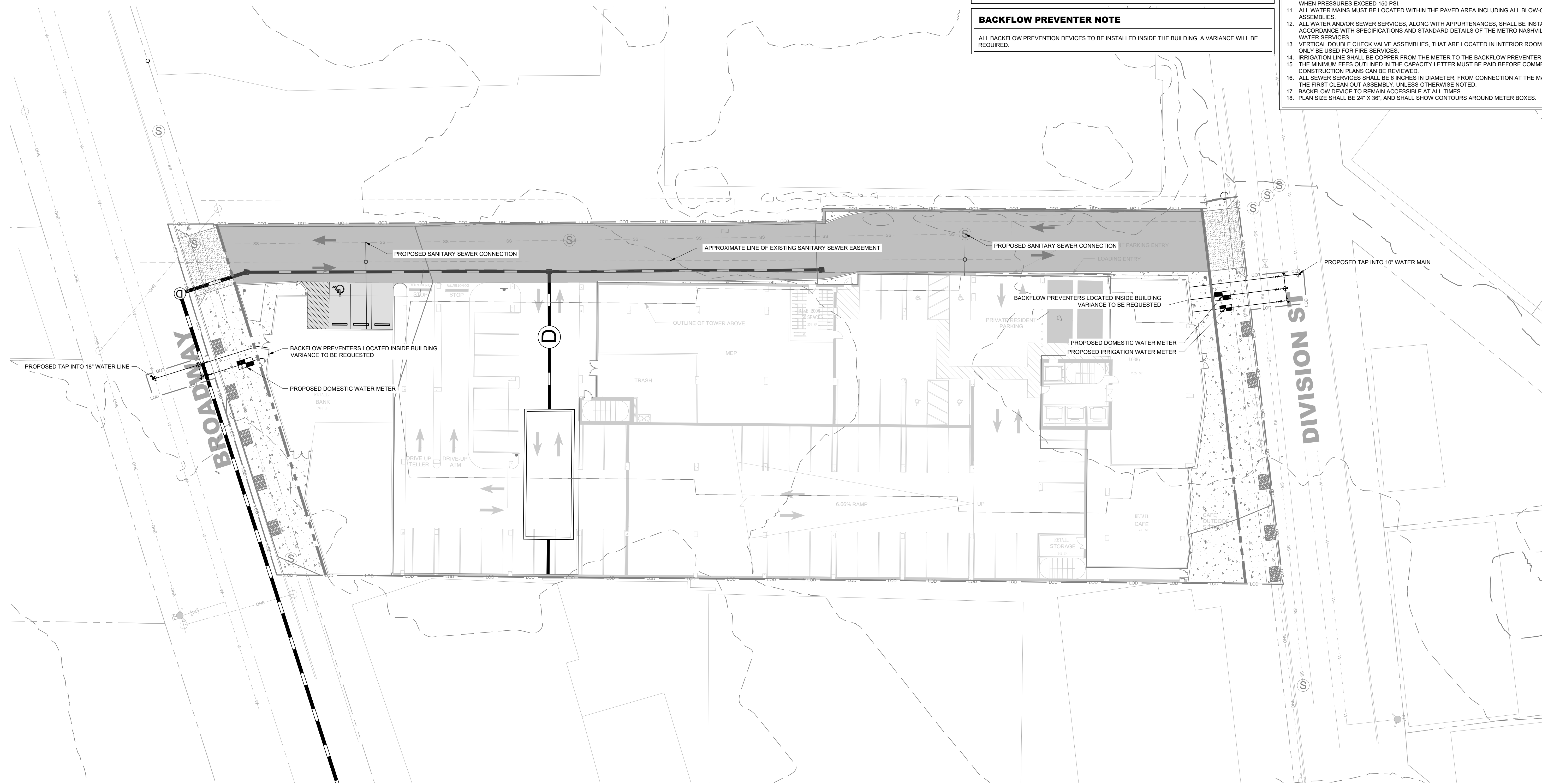
DESIGNED BY: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: 5/17/2022  
 KIMLEY-HORN PROJECT NO. 118345001

GRADING AND DRAINAGE PLAN - OVERALL  
 SHEET NUMBER  
**C4-00**





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**VALVE NOTE**

ALL VALVES SHALL BE INSTALLED IN METRO NASHVILLE WATER SERVICES APPROVED VALVE BOX AND COVER. COVER TO BE MARKED WITH "WATER".

**UTILITY NOTES**

1. CONTRACTOR RESPONSIBLE FOR VERIFYING EXISTING ELEVATIONS COMPARED TO THOSE SHOWN ON PLAN PRIOR TO GRADING. NOTIFY OWNER'S REPRESENTATIVE IF DISCREPANCIES ARE FOUND.
2. AREAS FOR CONSTRUCTION THAT REQUIRE DE-WATERING FOR EXCAVATION WILL BE DONE AT NO ADDITIONAL COST TO THE OWNER.
3. ALL WATER AND SEWER MATERIALS AND CONSTRUCTION SHALL BE IN COMPLIANCE WITH METRO NASHVILLE WATER SERVICES' STANDARD SPECIFICATIONS.
4. MAINTAIN MINIMUM 10 FEET HORIZONTAL SEPARATION BETWEEN WATER & SANITARY SEWER OR 18" VERTICAL SEPARATION AT CROSSING LOCATIONS.
5. REFER TO METRO NASHVILLE WATER SERVICES STANDARD SPECIFICATIONS FOR PIPE BEDDING REQUIREMENTS.
6. ALL WATER VALVES TO BE LOCATED OUTSIDE OF PAVED AREAS WHERE POSSIBLE.
7. THE CONTRACTOR SHALL PROVIDE A COMPLETE SET OF AS-BUILT DRAWINGS OF WATER & SANITARY SEWER CONSTRUCTION PRIOR TO THE ACCEPTANCE OF THE PUBLIC UTILITIES.
8. CONTRACTOR RESPONSIBLE FOR DISCONNECTING EXISTING WATER SERVICE LINES AT THE PUBLIC MAIN PER METRO WATER SERVICES STANDARDS. PAVEMENT REPAIR SHALL BE DONE IN ACCORDANCE TO METRO PUBLIC WORKS STANDARD.
9. ALL EXISTING UTILITIES SHALL BE CUT AND CAPPED AT MAIN.

**EXISTING UTILITIES NOTE**

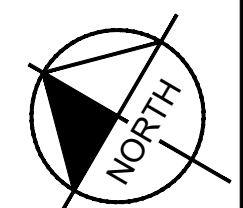
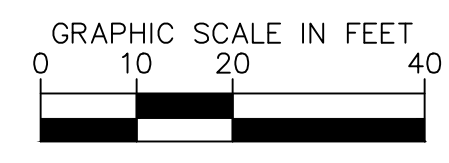
CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES AND/OR CONFLICTS WITH EXISTING OR PROPOSED UTILITIES PRIOR TO PROCEEDING.

**BACKFLOW PREVENTER NOTE**

ALL BACKFLOW PREVENTION DEVICES TO BE INSTALLED INSIDE THE BUILDING. A VARIANCE WILL BE REQUIRED.

**METRO NASHVILLE WATER SERVICES STANDARD NOTES**

1. ALL WATER AND SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
2. THE CONTRACTOR IS RESPONSIBLE FOR REIMBURSING THE METRO NASHVILLE WATER SERVICES THE COST OF INSPECTION.
3. THE CONTRACTOR IS TO PROVIDE AND MAINTAIN THE CONSTRUCTION IDENTIFICATION SIGN FOR PRIVATE DEVELOPMENT APPROVED.
4. AFTER COMPLETION OF THE SANITARY SEWER, THE DEVELOPER IS RESPONSIBLE FOR THE TELEVISIONING OF THE LINES PRIOR TO FINAL ACCEPTANCE. THE VIDEOTAPING MUST BE COORDINATED WITH THE METRO NASHVILLE WATER SERVICES INSPECTION SECTION. ALL COSTS WILL BE BORNE BY THE DEVELOPER.
5. ALL CONNECTIONS TO EXISTING MANHOLES SHALL BE BY CORING AND RESILIENT CONNECTOR METHOD.
6. REDUCED PRESSURE BACKFLOW PREVENTION DEVICES (RPBP) OR DUAL CHECK VALVE WILL BE REQUIRED ON ALL TEST AND FILL LINES (JUMPER) NEEDED FOR WATER MAIN CONSTRUCTION AND MUST BE APPROVED BY THE METRO NASHVILLE WATER SERVICES.
7. ALL WATER METERS SHALL BE A MINIMUM OF 24" NOT TO EXCEED A MAXIMUM OF 28" BELOW FINISHED GRADE.
8. UPON COMPLETION OF CONSTRUCTION OF WATER AND/OR SEWER, THE ENGINEER SHALL PROVIDE THE DEPARTMENT WITH A COMPLETE SET OF AS-BUILT PLANS ON MOIST ERASABLE MYLAR IN REVERSE AND IN DIGITAL (.DWG) FORMAT. SEWER PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE ACTUAL FIELD ANGLES BETWEEN LINES, ALL ACTUAL SERVICE LINES AND TEE LOCATIONS, THE DISTANCE OF THE END OF THE SERVICE LINE TO PROPERTY CORNERS AND LINES AND/OR STATION AND OFFSET FROM SEWER CENTERLINE TO END OF SERVICE LINE, THE DEPTH TO THE TOP OF THE END OF THE SERVICE LINE, AND SHALL REFLECT ALL ALIGNMENT AND GRADE CHANGES. WATER LINE PLANS SHALL BE SEALED BY A LICENSED PROFESSIONAL ENGINEER OR A REGISTERED LAND SURVEYOR AND SHALL INCLUDE OFFSET DISTANCE FROM THE ROADWAY CENTERLINE, OR PROPERTY LINE RIGHT OF WAY, LINE DEPTH, LOCATIONS OF HYDRANTS, VALVES, REDUCERS, TEES AND PRESSURE REDUCING DEVICES WHERE APPLICABLE. ALL DRAWINGS MUST BE COMPLETED AND SUBMITTED PRIOR TO ACCEPTANCE OF THE SEWERS OR WATER MAINS INTO THE PUBLIC SYSTEM AND ANY CONNECTIONS BEING MADE.
9. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE CUSTOMER SIDE OF THE METER WHEN PRESSURES EXCEED 100 PSI.
10. PRESSURE REGULATING DEVICES WILL BE REQUIRED ON THE STREET SIDE OF THE METER WHEN PRESSURES EXCEED 150 PSI.
11. ALL WATER MAINS MUST BE LOCATED WITHIN THE PAVED AREA INCLUDING ALL BLOW-OFF ASSEMBLIES.
12. ALL WATER AND/OR SEWER SERVICES, ALONG WITH APPURTENANCES, SHALL BE INSTALLED IN ACCORDANCE WITH SPECIFICATIONS AND STANDARD DETAILS OF THE METRO NASHVILLE WATER SERVICES.
13. VERTICAL DOUBLE CHECK VALVE ASSEMBLIES, THAT ARE LOCATED IN INTERIOR ROOMS, CAN ONLY BE USED FOR FIRE SERVICES.
14. IRRIGATION LINE SHALL BE COPPER FROM THE METER TO THE BACKFLOW PREVENTER.
15. THE MINIMUM FEES OUTLINED IN THE CAPACITY LETTER MUST BE PAID BEFORE COMMERCIAL CONSTRUCTION PLANS CAN BE REVIEWED.
16. ALL SEWER SERVICES SHALL BE 6 INCHES IN DIAMETER, FROM CONNECTION AT THE MAIN UNTIL THE FIRST CLEAN OUT ASSEMBLY, UNLESS OTHERWISE NOTED.
17. BACKFLOW DEVICE TO REMAIN ACCESSIBLE AT ALL TIMES.
18. PLAN SIZE SHALL BE 24" X 36", AND SHALL SHOW CONTOURS AROUND METER BOXES.



**DRAFT  
 PRELIMINARY  
 PLANS  
 FOR REVIEW ONLY**

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DESIGNED BY: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: 5/17/2022  
 KIMLEY-HORN PROJECT NO. 118345001

UTILITY PLAN - OVERALL  
 SHEET NUMBER  
**C6-00**



# 1711 BROADWAY

2022SP-027-001 SP Plan | May 17, 2022





# SP General Notes

*\* If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUG-A-NS zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.*

*\* If required, notice information is prepared and sent to applicants based on the plan and information submitted with the initial submittal. Should your plans change significantly with the resubmittal, it may be necessary for revised notice information to be prepared and sent. This could cause a delay and may result in deferral.*

*\* The Specific Plan process is a multi-step process. If the Preliminary SP is approved, there are additional steps required prior to issuance of a building permit. These steps include: final site plan and building permit review. The final site plan is submitted through the normal Planning Commission review process. For building permit review, when applying for a building permit you must submit 3 copies of plans and a review fee of \$250 directly to the Planning front counter for processing. We do not need the complete building permit set. Please submit only site plan, landscaping plan, grading plan, and building elevations*

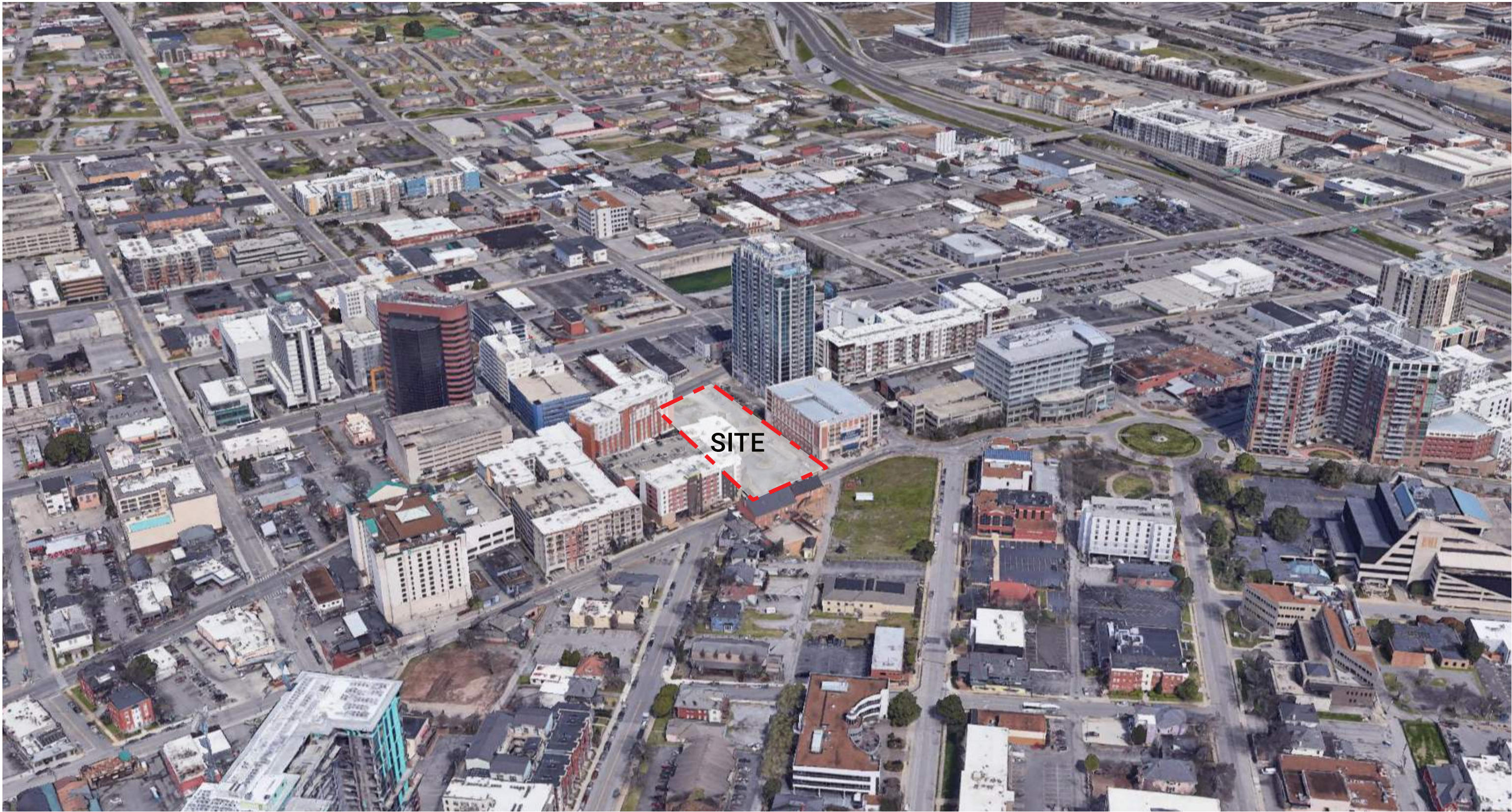
*\* Limited building detail, and/ or building construction information provided. Construction must meet all applicable building and fire codes. Any additional fire code or access issues will be addressed during the construction permitting process. Future development or construction may require changes to meet adopted fire and building codes.*

*\* Final constructions plans shall comply with the design regulations established by NDOT. Final design and improvements may vary based on actual field conditions. A private hauler is required for site waste/recycle disposal. There are to be no vertical obstructions in new public sidewalks. Provide 24 ft. width for both drive accesses.*

*\*All façade materials, exclusive of clear fenestration, shall be high quality and selected from the following list: masonry, masonry panels, textured metal, metal paneling, precast concrete, precast concrete panel, fiber cement panel, fiber cement siding, spandrel glass (on upper stories only), or materials substantially similar in form and function. This requirement applies to all facades.*



# Aerial Context





# Site









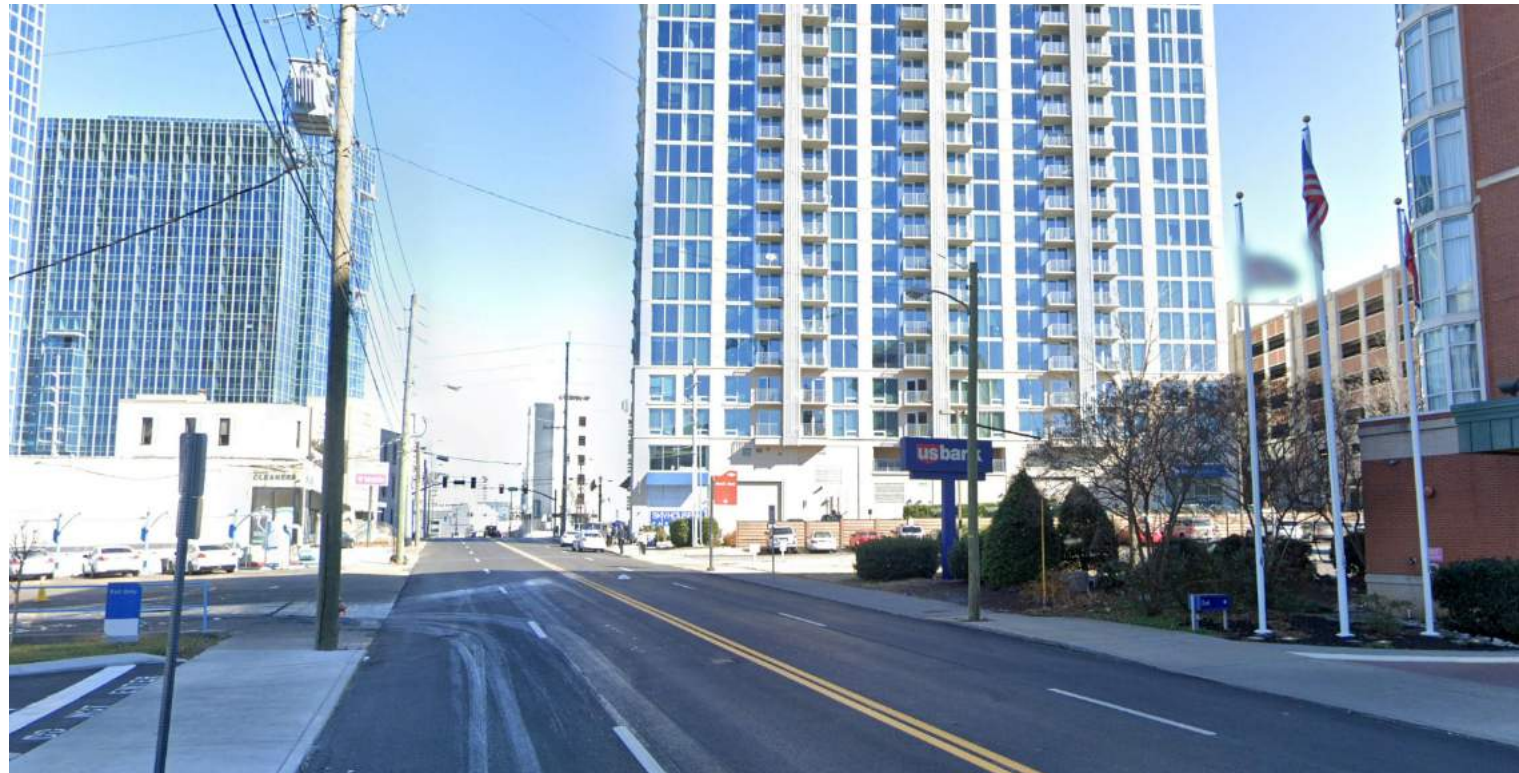
# Existing Site Conditions



**BROADWAY - LOOKING WEST**



**DIVISION ST - LOOKING WEST**



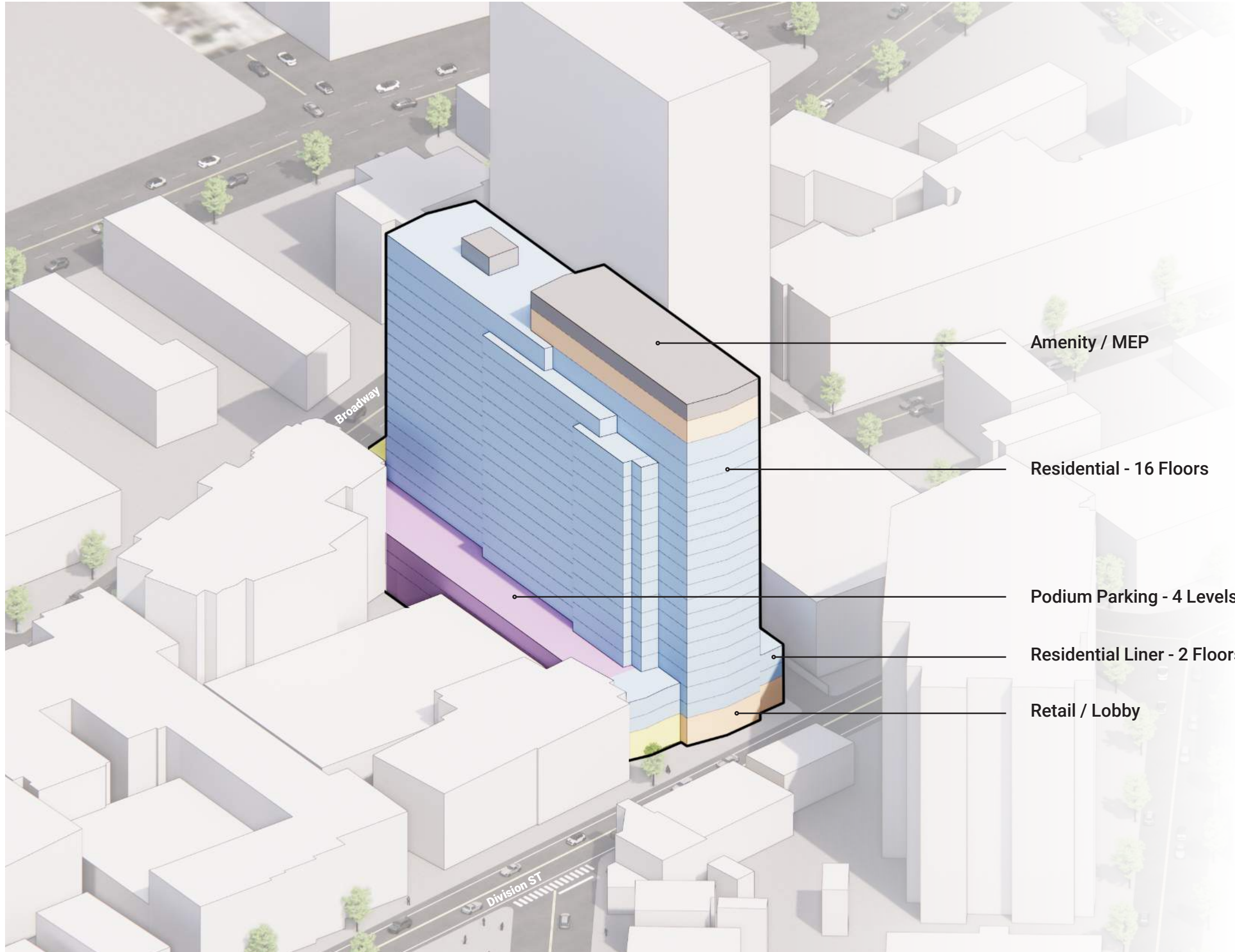
**BROADWAY - LOOKING EAST**



**DIVISION ST - LOOKING EAST**



# Program



Typical Residential Floor GFA:	599,000 SF
Total Residential Floor NSA:	338,400 SF
Total Residential Units:	396 Units
Retail GFA:	6,100 SF
Total GFA:	605,100 SF
Total NSA:	344,500 SF
Total Levels:	20
Tower Height:	245' 6"

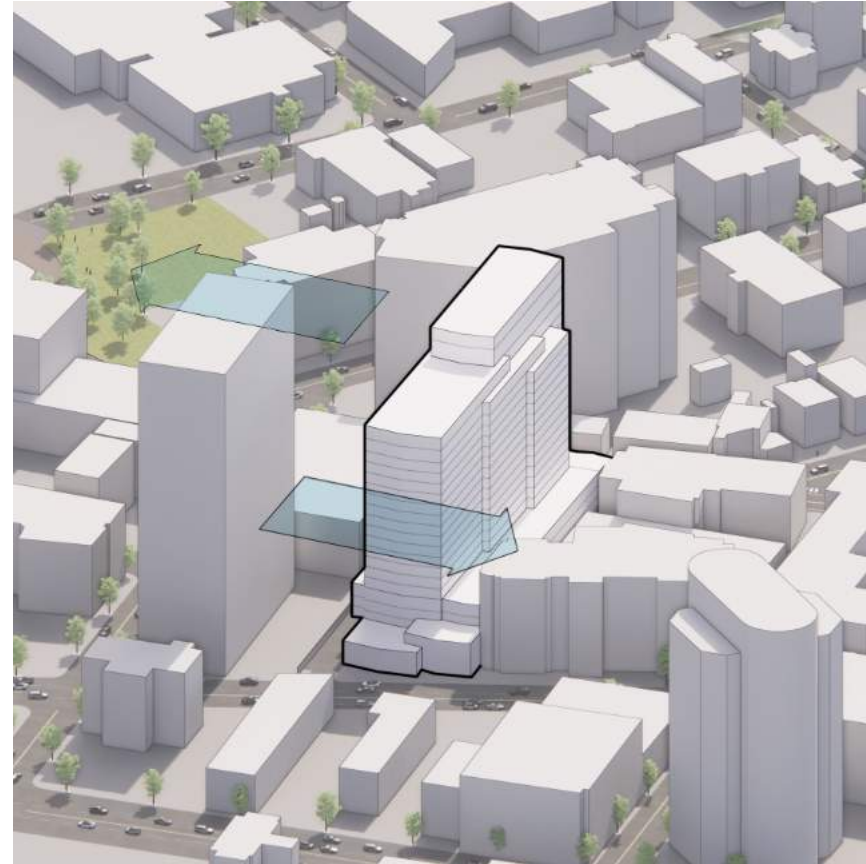


# Contextual Design



Activate Urban Street

Broadway and Division Street is activated with street fronts, approximately 6,000 sf of retail and spacious lobby.



Tower Placement

Tower is placed away from the existing residential tower as friendly development to it's neighbor.



Integrated Independent Properties

Two independent properties integrated into one design while allowing separate phase of construction.





*Honoring the historic Music Row district through rhythmic, graceful, and simple design*









ALUMINUM FIN (LIGHT BRONZE)

METAL PANEL (BRONZE)





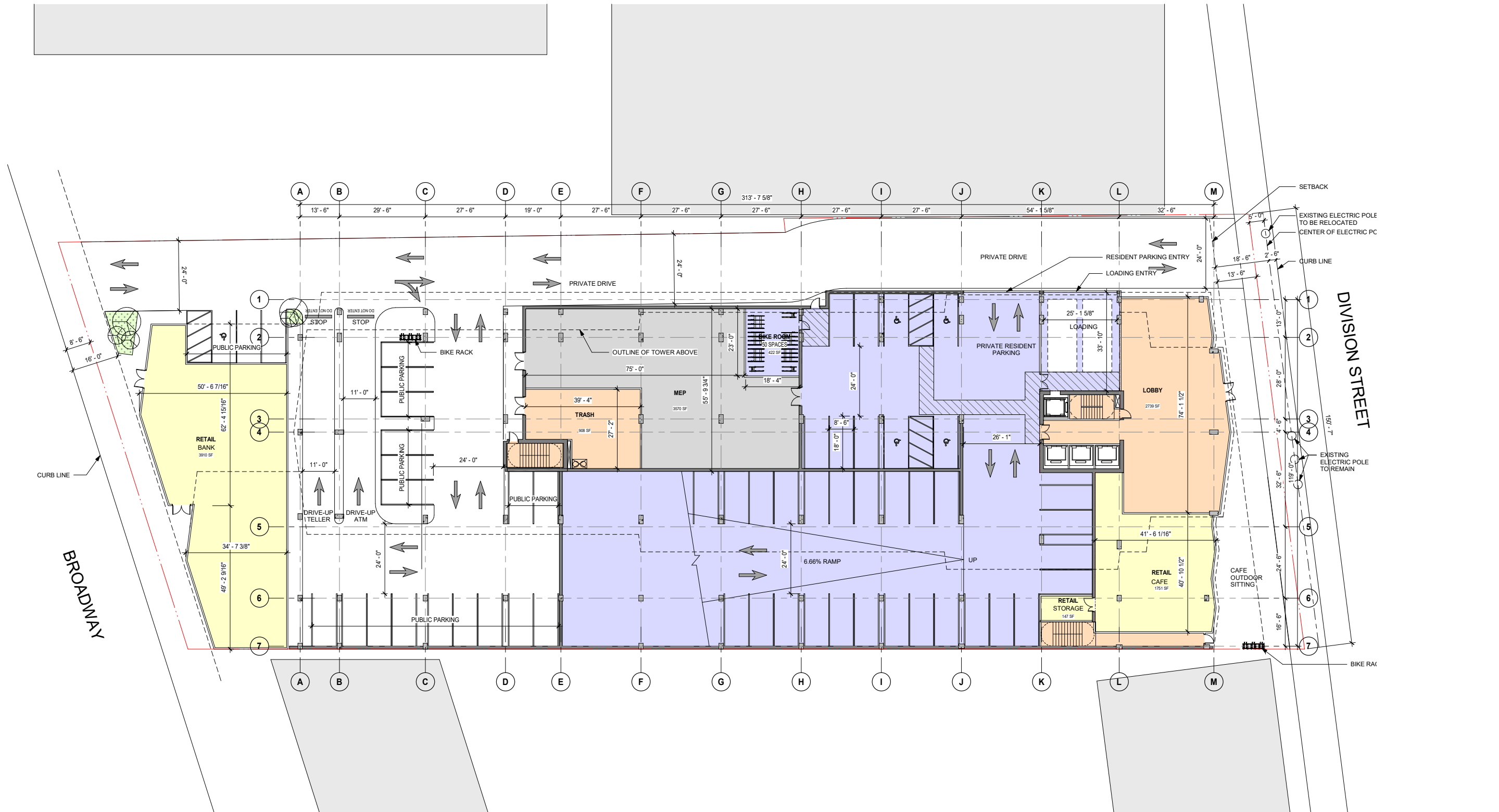




- METAL PANEL (BRONZE)
- PERFORATED METAL PANEL SCREEN (SILVER)
- PERFORATED METAL PANEL SCREEN (BRONZE)
- ALUMINUM FIN (LIGHT BRONZE)

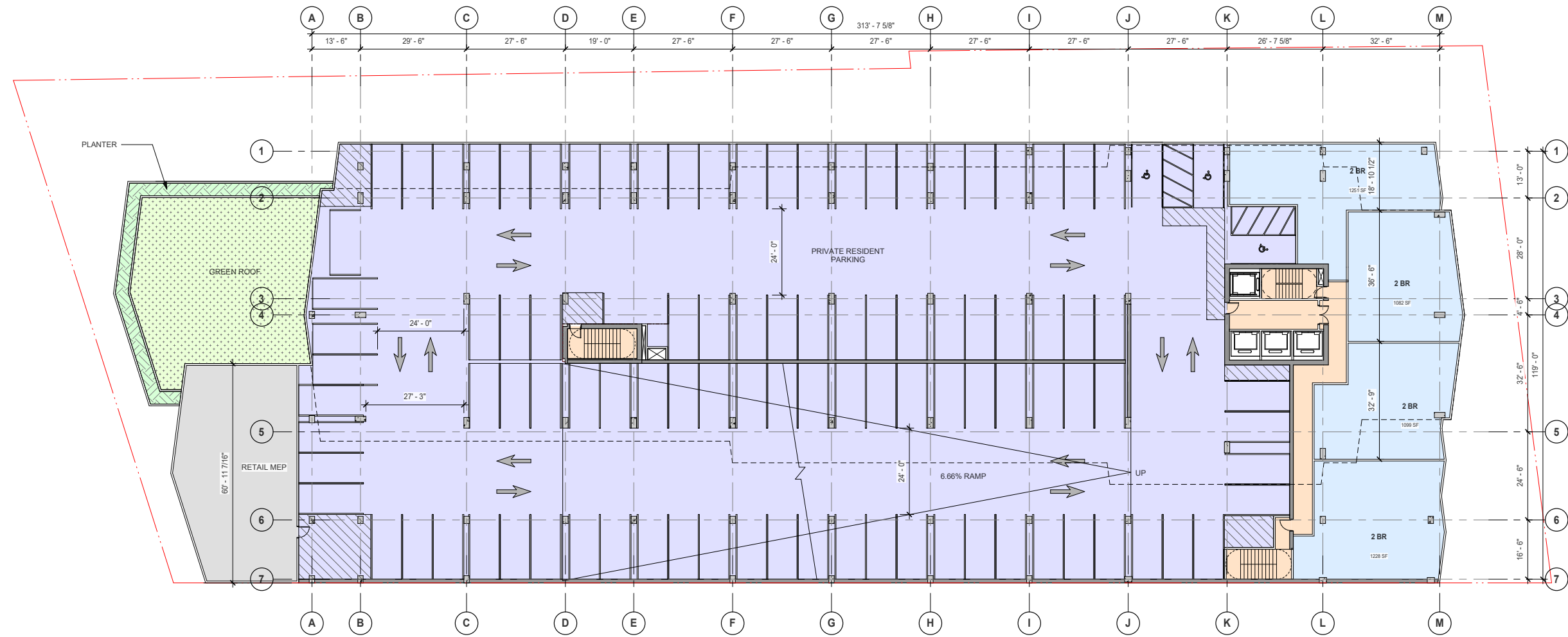


# Ground Floor Plan



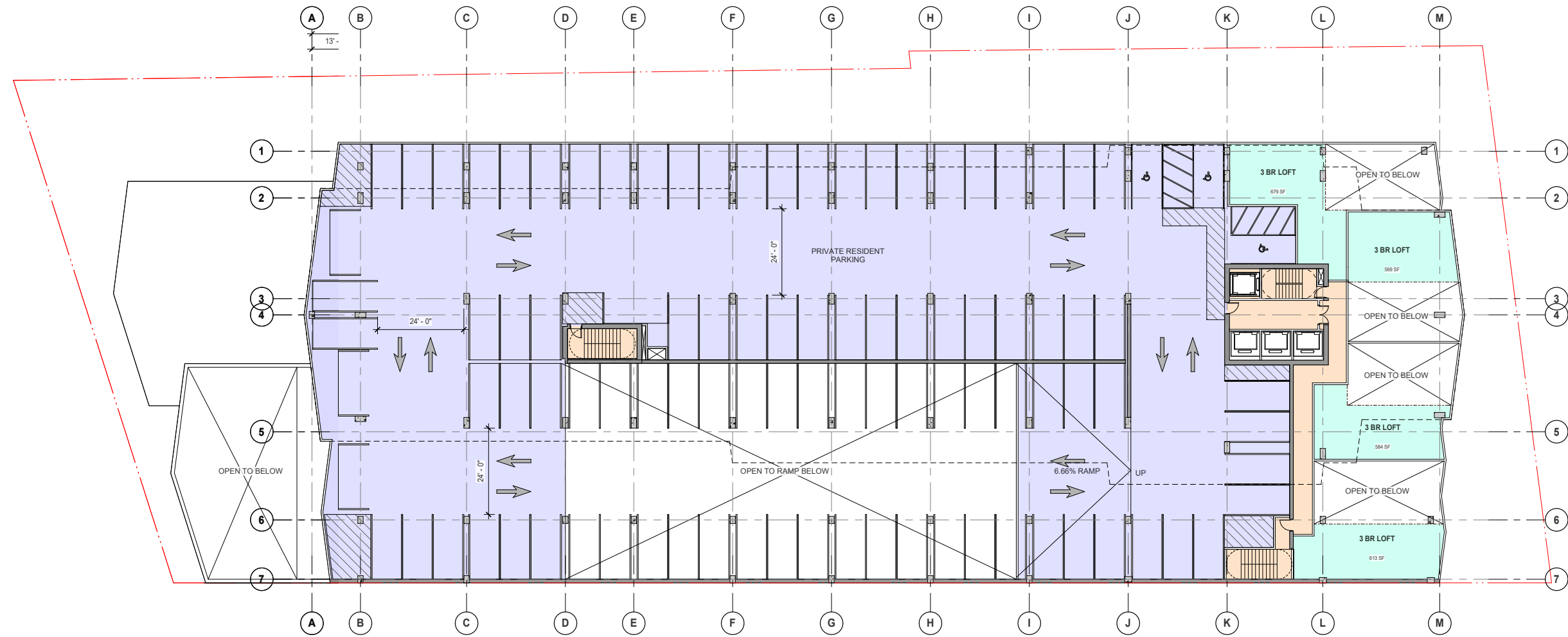


# Typical Parking Floor Plan (L1M - L3)



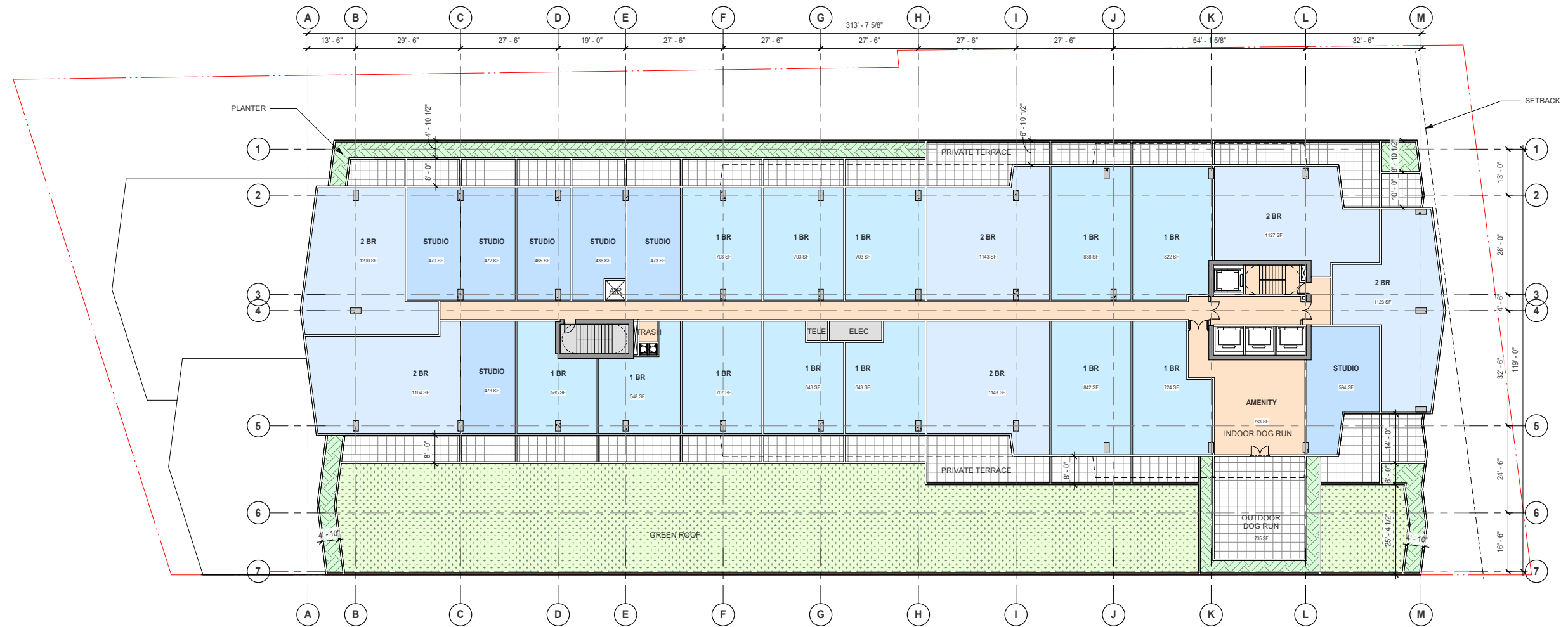


# L3 Mezzanine Floor Plan



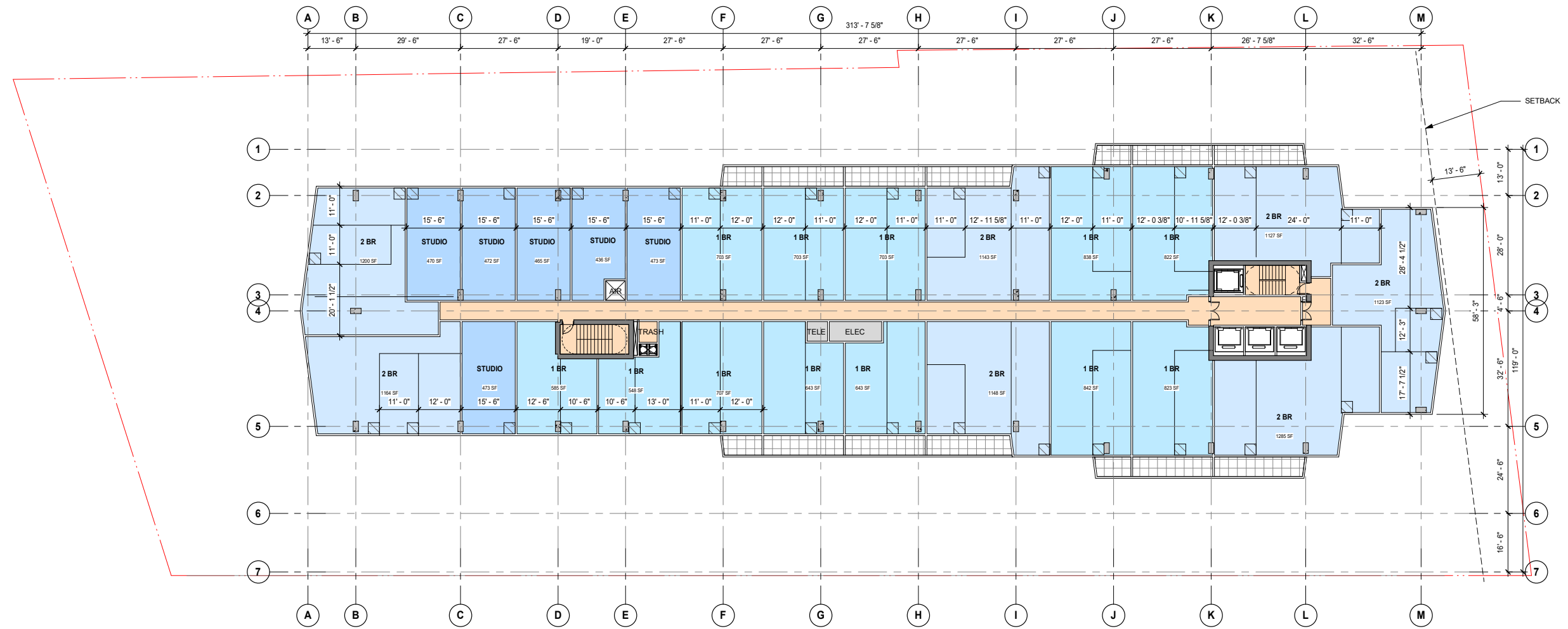


# L4 Floor Plan



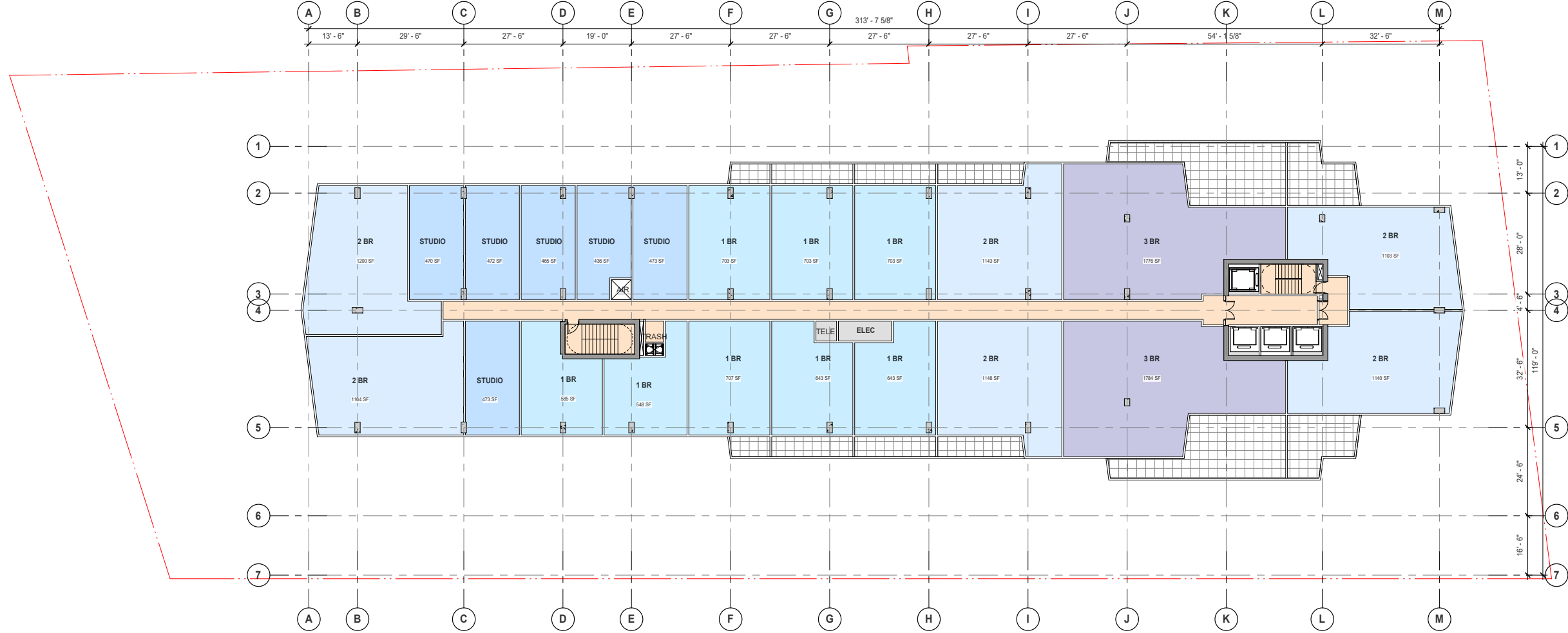


# Typical Floor Plan (L5 to L17)



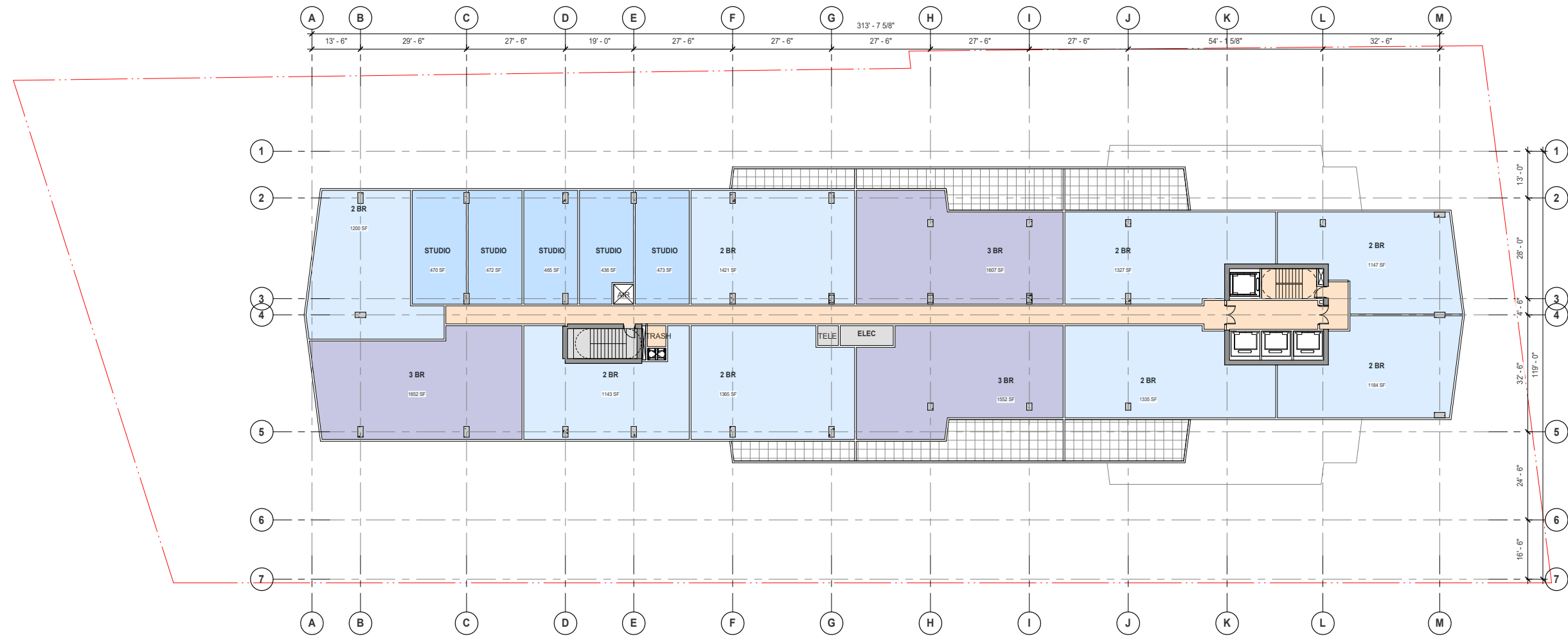


# L18 Floor Plan



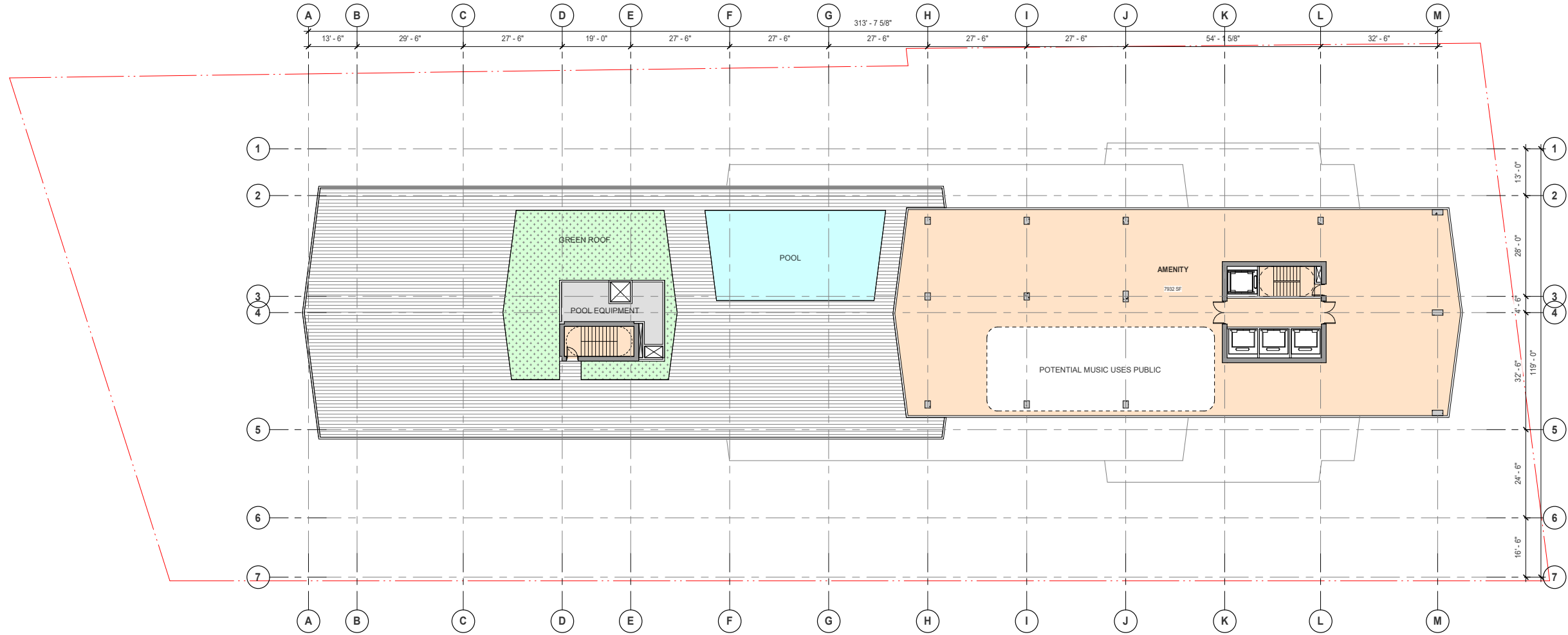


# L19 Floor Plan





# L20 Floor Plan





# East Elevation

*\*All façade materials, exclusive of clear fenestration, shall be high quality and selected from the following list: masonry, masonry panels, textured metal, metal paneling, precast concrete, precast concrete panel, fiber cement panel, fiber cement siding, spandrel glass (on upper stories only), or materials substantially similar in form and function. This requirement applies to all facades.*

*\*The architecture of the screening of the podium parking shall vertically integrate with the residential portion of the tower*

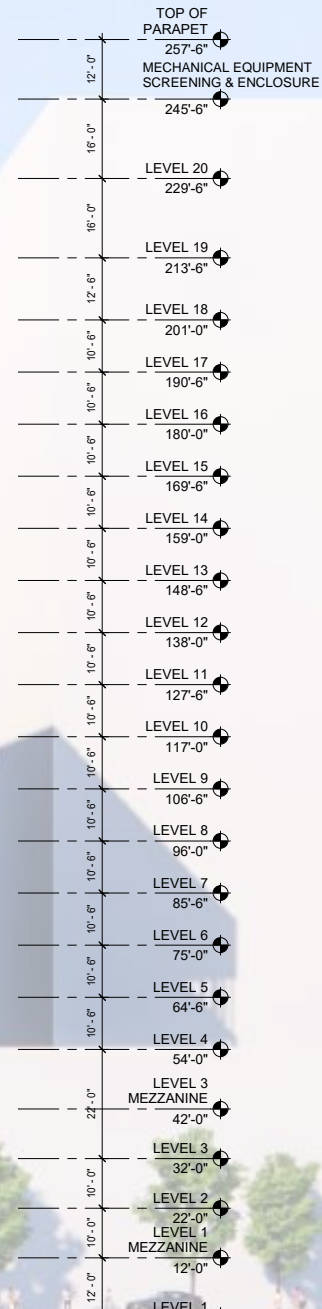




# South and North Elevations



South Elevation



*\*All façade materials, exclusive of clear fenestration, shall be high quality and selected from the following list: masonry, masonry panels, textured metal, metal paneling, precast concrete, precast concrete panel, fiber cement panel, fiber cement siding, spandrel glass (on upper stories only), or materials substantially similar in form and function. This requirement applies to all facades.*

*\*The architecture of the screening of the podium parking shall vertically integrate with the residential portion of the tower*



North Elevation



# West Elevation

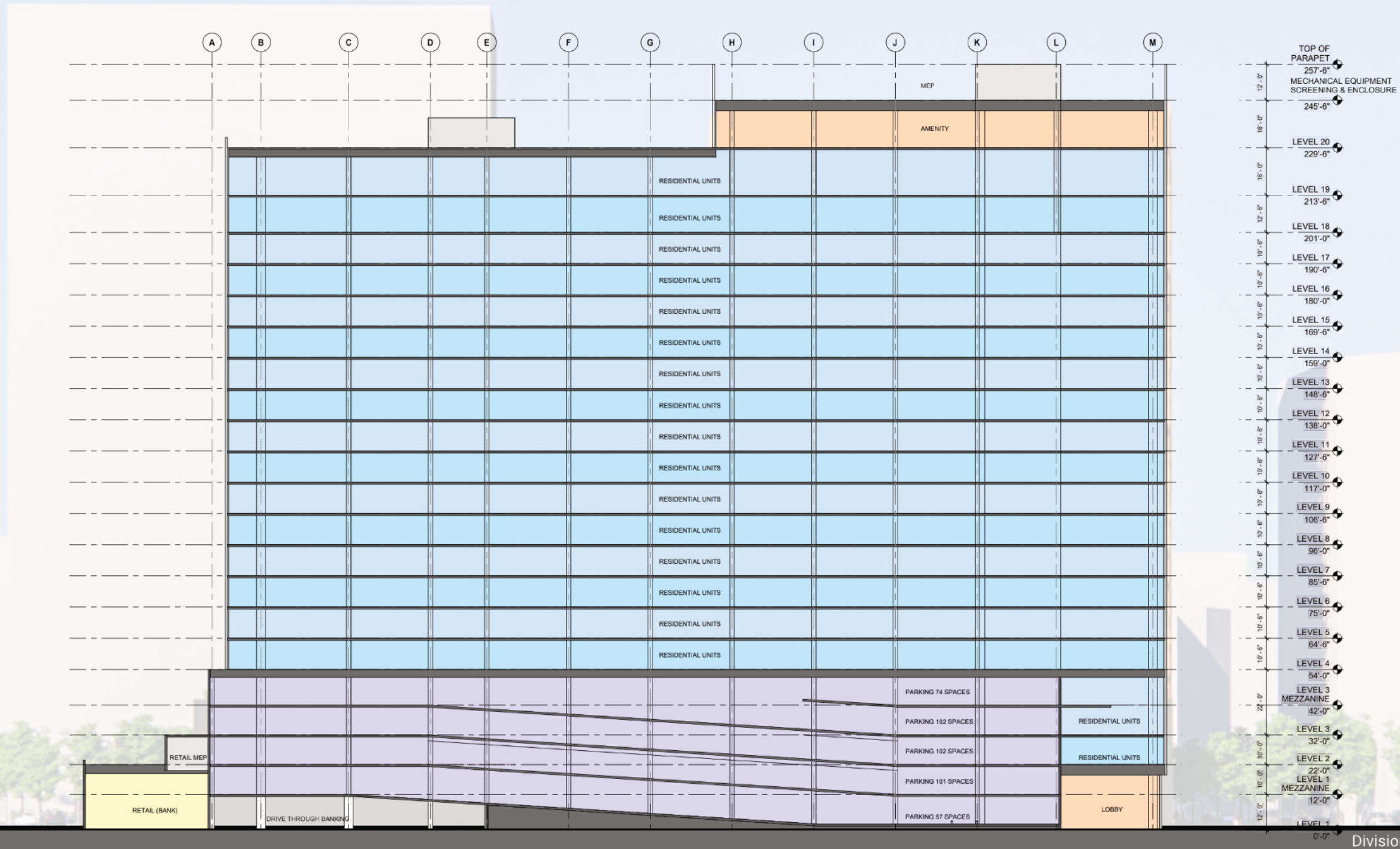
*\*All façade materials, exclusive of clear fenestration, shall be high quality and selected from the following list: masonry, masonry panels, textured metal, metal paneling, precast concrete, precast concrete panel, fiber cement panel, fiber cement siding, spandrel glass (on upper stories only), or materials substantially similar in form and function. This requirement applies to all facades.*

*\*The architecture of the screening of the podium parking shall vertically integrate with the residential portion of the tower*





# Section



Broadway

Division ST



# Area Matrix

1711 BROADWAY NASHVILLE - AREA MATRIX - 05-17-2022														
FLOOR	RESIDENTIAL				RETAIL	OFFICE	PARKING			MECHANICAL	AREAS		HEIGHTS	
	TOTAL UNITS	COMMON	RENTABLE	TERRACE			PARKING SPACES	BICYCLES	PARKING AREA		TOTAL GROSS AREA	F.A.R.	FLOOR TO FLOOR HEIGHT	OVERALL HEIGHT
ROOF	mech									3,000	3,000	0	12'-0 "	257'-6 "
20	Amenity	9,651		10,990							20,641	9,651	16'-0 "	245'-6 "
19	16	2,492	18,180	2,325							22,997	20,672	16'-0 "	229'-6 "
18	22	2,492	19,525	2,840							24,857	22,017	12'-6 "	213'-6 "
17	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	201'-0 "
16	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	190'-6 "
15	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	180'-0 "
14	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	169'-6 "
13	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	159'-0 "
12	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	148'-6 "
11	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	138'-0 "
10	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	127'-6 "
9	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	117'-0 "
8	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	106'-6 "
7	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	96'-0 "
6	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	85'-6 "
5	25	2,492	20,675	1,692							24,859	23,167	10'-6 "	75'-0 "
4	25	3,345	19,820	15,350							38,515	23,165	10'-6 "	64'-6 "
3M	4		2,400				71		24,445		26,845	2,400	12'-0 "	54'-0 "
3		1,685	4,867				99		31,970		38,522	6,552	10'-0 "	42'-0 "
2	4	1,685	4,867				102		32,400	960	39,912	7,512	10'-0 "	32'-0 "
1M		1,120					101		32,300		33,420	1,120	10'-0 "	22'-0 "
1		4,930			6,100		70	400	17,970	3,835	32,835	14,865	12'-0 "	12'-0 "
<b>TOTAL</b>	<b>396</b>	<b>59,796</b>	<b>338,434</b>	<b>53,501</b>	<b>6,100</b>		<b>443</b>	<b>400</b>	<b>139,085</b>	<b>7,795</b>	<b>604,711</b>	<b>409,125</b>		

SITE AREA	57,234
AVERAGE UNIT SIZE	855
TYPICAL FLOOR EFFICIENCY	89.24%
TOTAL USED FAR	<b>7.15</b>

gross area includes parking and landscaped terraces  
Loading is included in parking number



# Unit mix and Parking Calculation

UNIT MIX MATRIX						
Floor	STUDIO	1 BED	2 BED	3 BED		TOTAL
20						
19	5		8	3		16
18	6	8	6	2		22
17	6	12	7			25
16	6	12	7			25
15	6	12	7			25
14	6	12	7			25
13	6	12	7			25
12	6	12	7			25
11	6	12	7			25
10	6	12	7			25
9	6	12	7			25
8	6	12	7			25
7	6	12	7			25
6	6	12	7			25
5	6	12	7			25
4	7	12	6			25
3				4		4
2			4			4
<b>TOTAL</b>	<b>96</b>	<b>176</b>	<b>115</b>	<b>9</b>		<b>396</b>
Percentage	<b>24%</b>	<b>44%</b>	<b>29%</b>	<b>2%</b>		<b>100%</b>

	STUDIO	1 BED	2 BED	3 BED			Residential	10% Reduction (Transit)		Retail 10% Reduction (Transit)
Parking Required per Table 17.20.030 for UZO district	96	176	172.5	13.5			458	412		14
Provided								423		20



