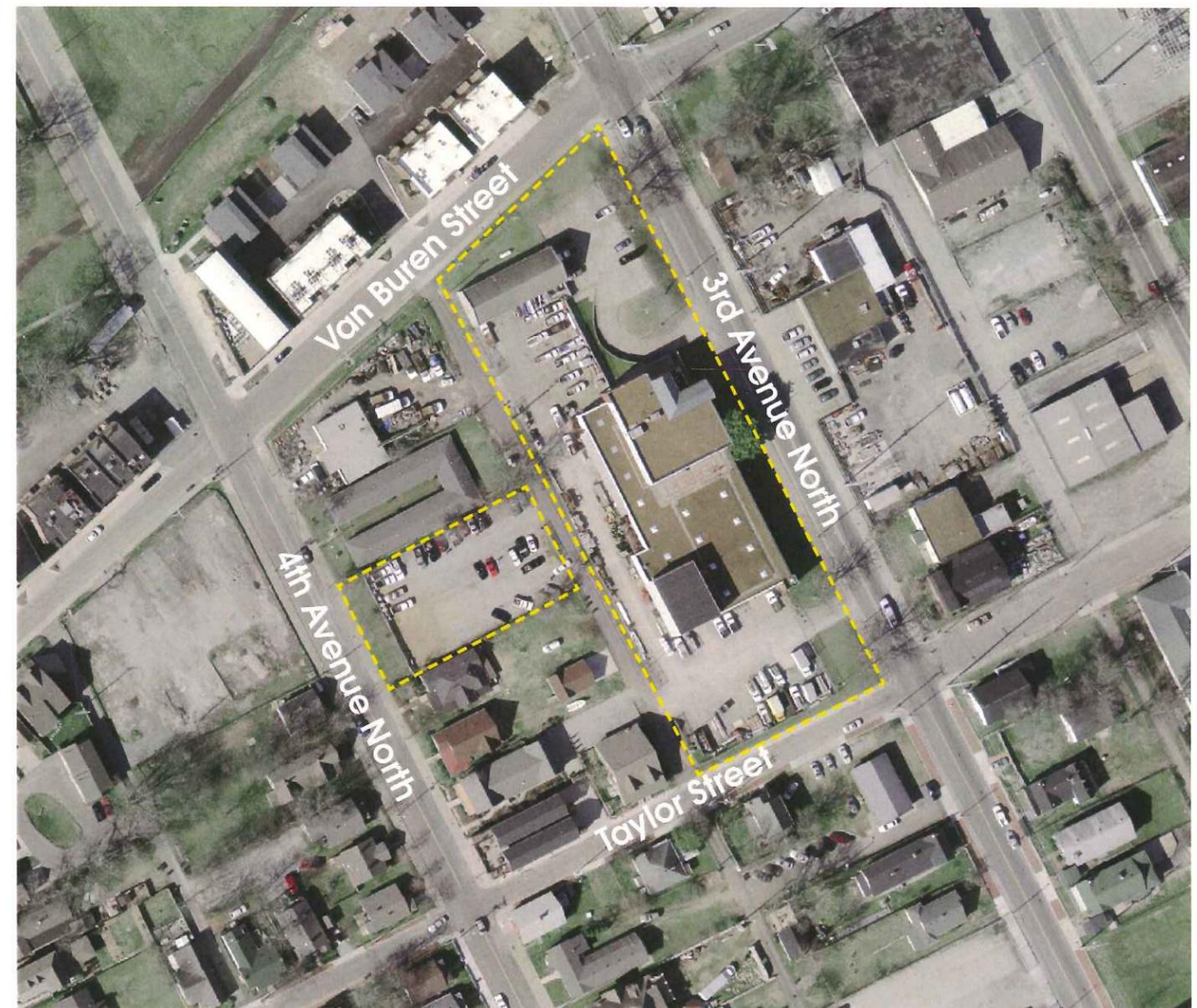
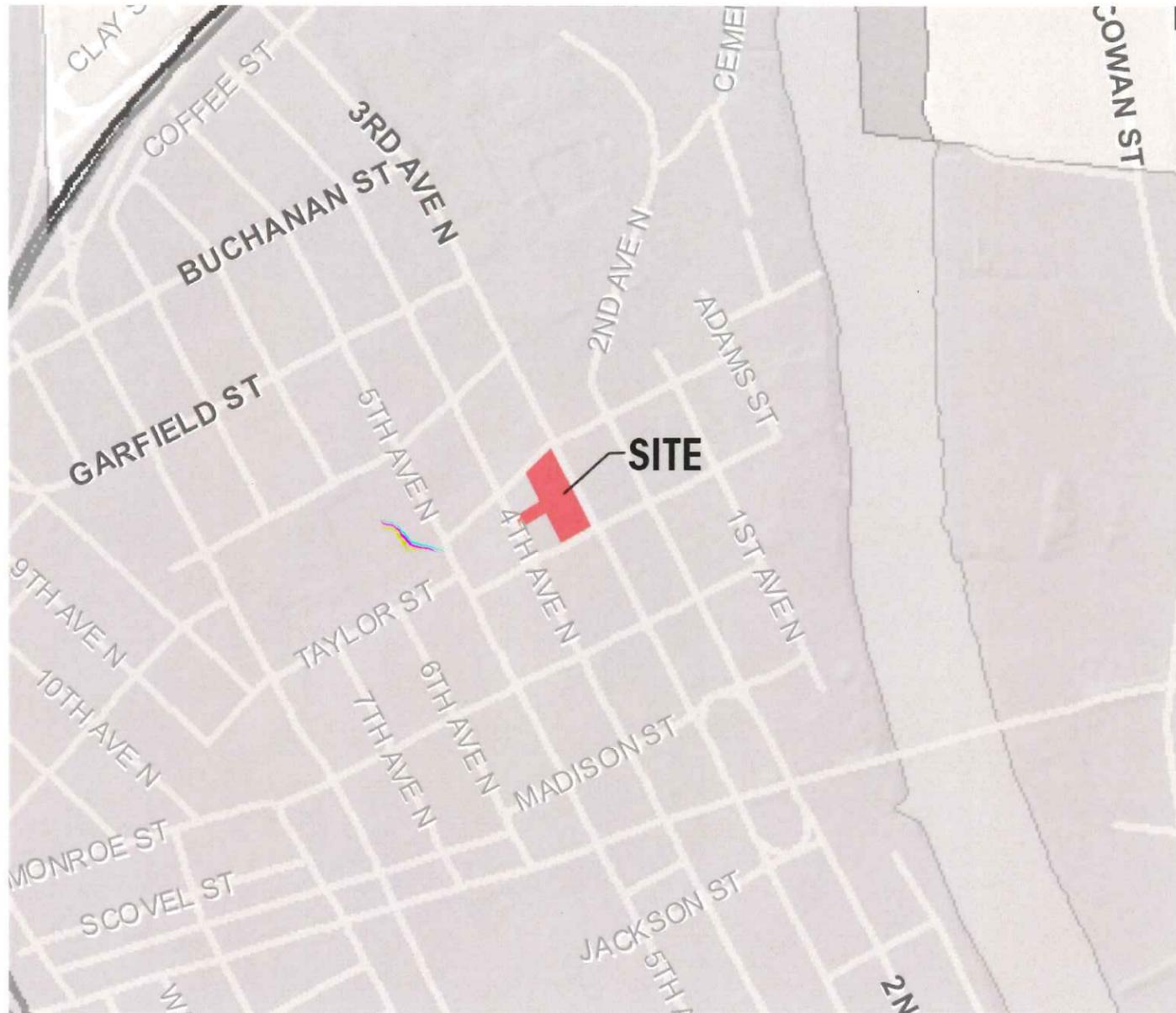


# Fountains Germantown



Preliminary SP  
MPC Case Number- 2014 SP - 020 - 001  
March 18, 2014





**Site Info**

Site is 2.43 acres of land located:

1401-1407 3rd Avenue North  
1408-1410 4th Avenue

Map 82-09, Parcels 104, 105, 111, 112 and 113

Council District 19 - Erica Gilmore

**Owners of Record**

R.D. Herbert and Sons Company  
1407 3rd Avenue North  
Nashville, TN 37208

Houston Herbert  
615-627-0222

**Developer**

Proffitt Dixon Partners  
Stuart Proffitt  
704-817-9126  
stuart@proffittdixon.com



**Civil Engineer**

Civil Site  
Kevin Gangaware  
615-248-9999  
keving@civil-site.com

**Landscape Architect**

LandDesign  
Heth Kendrick  
704-333-0325  
hkendrick@landdesign.com

**Land Planner / Architect**

Smith Gee Studio  
Scott Morton  
615-739-5555  
smorton@smithgeestudio.com

Site Info

Site is 2.43 acres of land located:

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Council District 19 - Erica Gilmore

Current Zoning: Industrial Restrictive (IR)

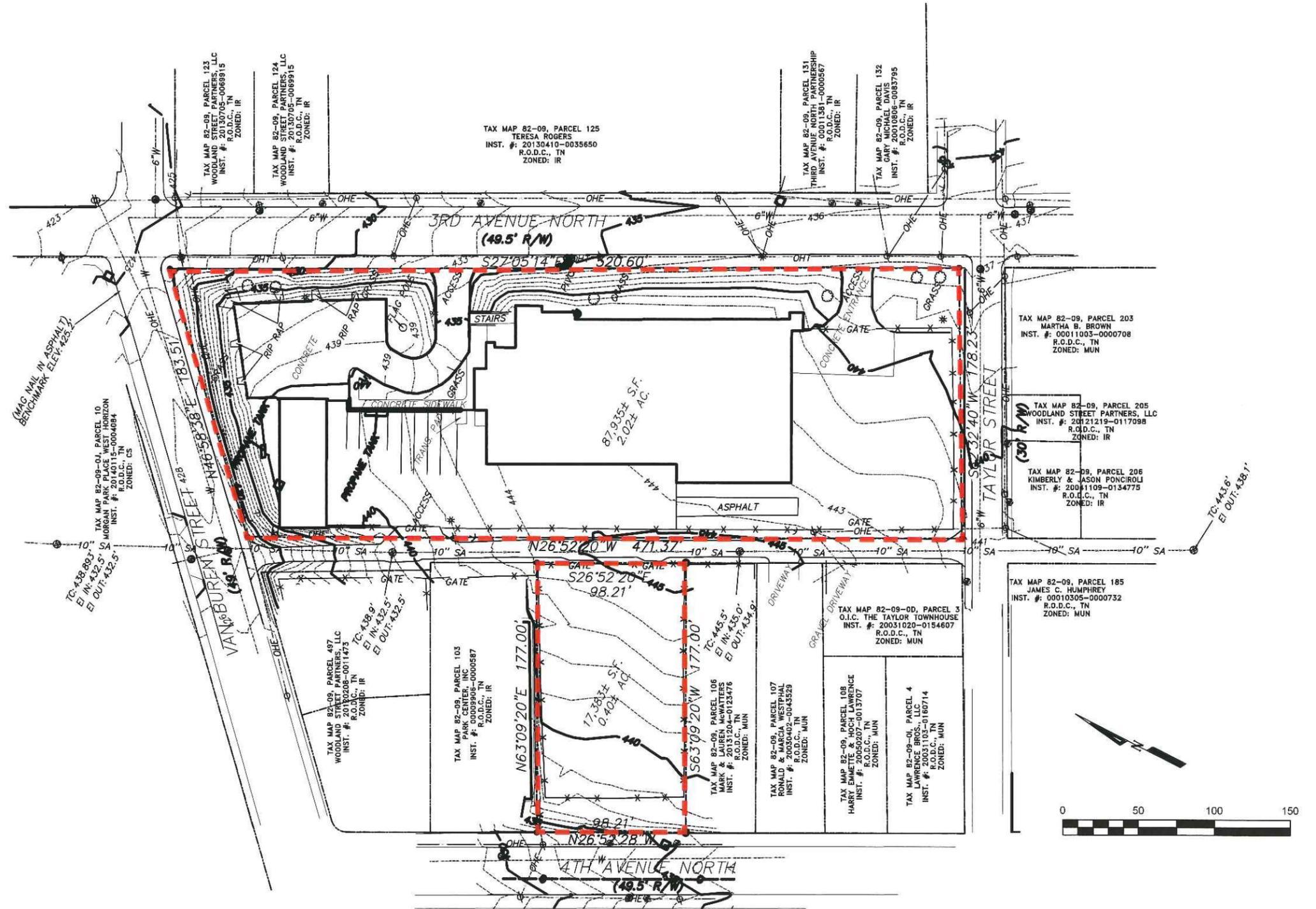
Land Use Policy: T4 Urban Mixed Use Neighborhood

Special Policy: 08-T4-MU-01

Overlays:

Germantown Historic Overlay

MDHA Phillips Jackson Redevelopment District (4th Ave. Property Only)



**General Plan Consistency**

The property is located within Subarea 8 of the North Nashville Community Plan. The land use policy envisioned for the property is T4 Urban Mixed Use Neighborhood. This policy is utilized as a transition between more intense and less intense community character policies and a unique environment where a variety of uses are encouraged. Additionally, the site is located within a special policy area 08-T4-MU-01 which states:

*Structures on 3rd Avenue from Jefferson Street to Monroe Street and on 2nd Avenue and parts of 3rd Avenue North of Taylor Street should generally have a maximum height of four (4) to six (6) stories, but should be no more than 75 feet.*

The enclosed SP application provides a plan that meets the vision of the community and is consistent with the T4 Urban Mixed Use Neighborhood policy.

**Mixed Use Neighborhood Characteristics**

- High Density housing
- Buildings built to back edge of sidewalk
- Parking located behind or beside buildings
- Moderate to high lot coverage
- Stoops and Porches common
- Friendly Pedestrian Environment
- Transition in scale to adjacent development
- Corner Focal Points
- Building entrances oriented to the street

**Germantown Historic Overlay Consistency**

The site is located on the Northeastern edge of the historic overlay and is considered new construction where there is minimal historical context.

**Overlay General Principles**

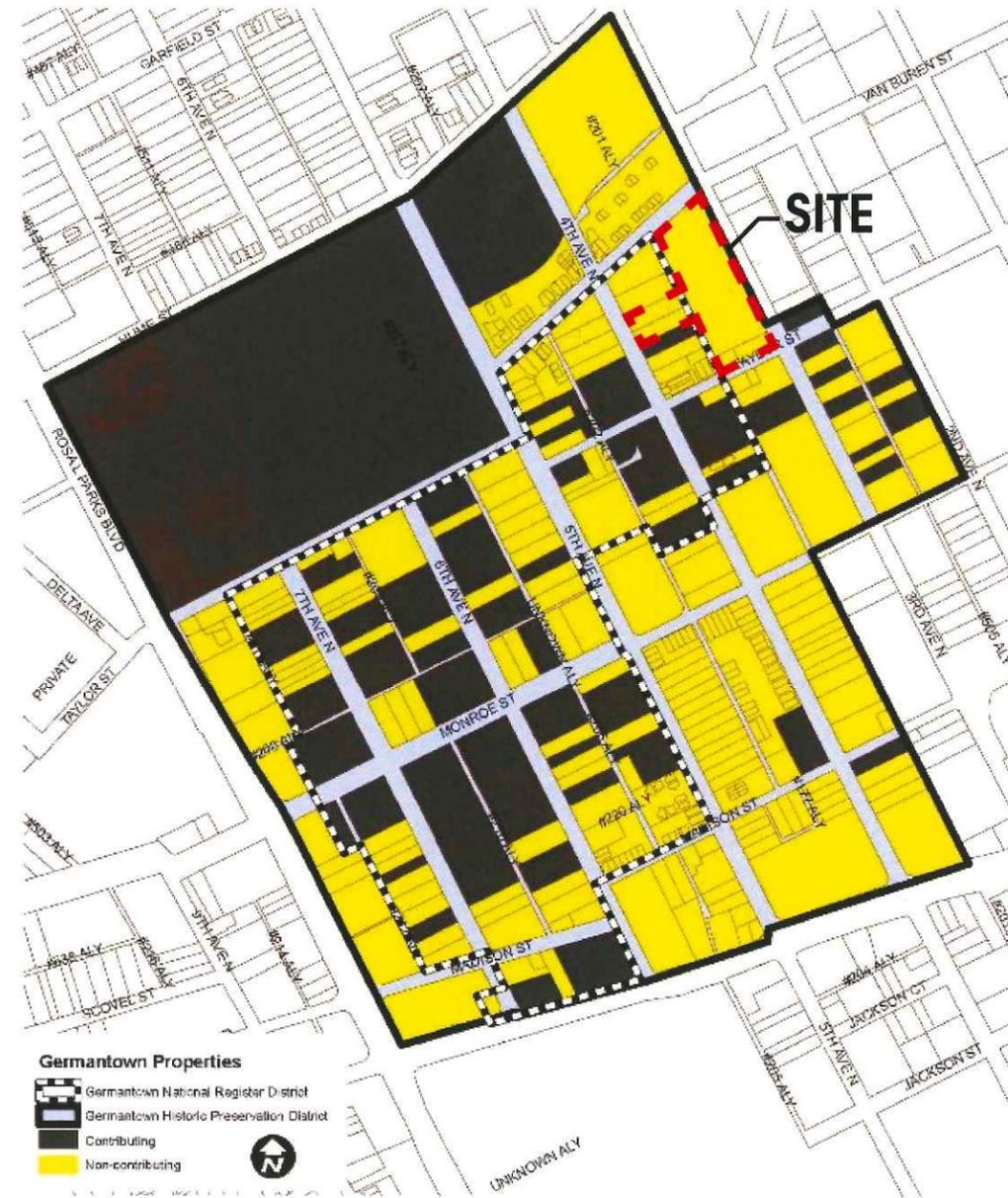
- Building should be sited and be stylistically respectful of their context
- Buildings should relate to pattern and rhythm of development consistent with a mixed-use urban neighborhood
- New projects should create a sense of place both internally and externally
- Ground floors should encourage pedestrian activity
- Building shall have small setbacks relative to the context of the community
- Setback from alley shall be a minimum of 5 feet in order to retain urban street character
- New structures shall avoid large expanses of unbroken facade planes
- Building Height:

*The mass and scale of new buildings will be reviewed relative to use and location within the District. Generally taller more massive structures are anticipated at the edges where Commercial Corridors (Jefferson Street and Rosa L. Parks Boulevard) bound the District. Lower height, smaller scale and less massive structures are predominant at the interior of the District. Third Avenue North is unique in the fact that it is an arterial passing through the eastern part of the District connecting downtown to Metro Center. Therefore as a connecting street with potentially higher traffic volumes more commercial uses, greater densities and taller heights may be appropriate. These guidelines and the Design Review Process are intended to provide a balance between the development potential of a particular site and compatibility of existing and adjacent buildings.*

The enclosed SP meets the intent of the Germantown Historic Overlay Guidelines and all applicable standards and principles.

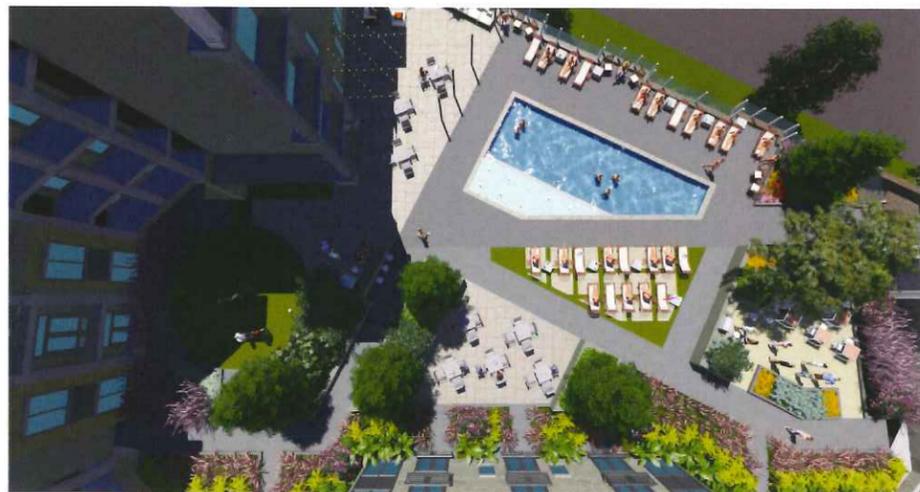
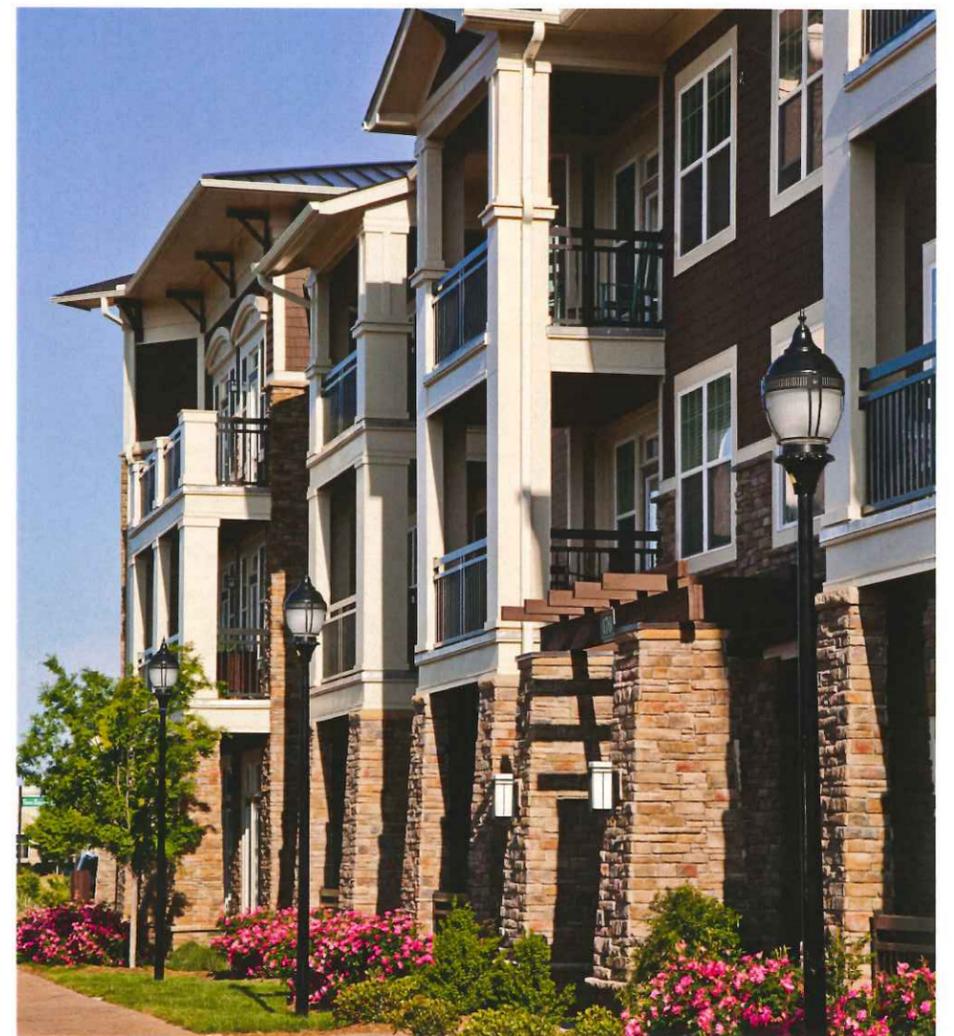
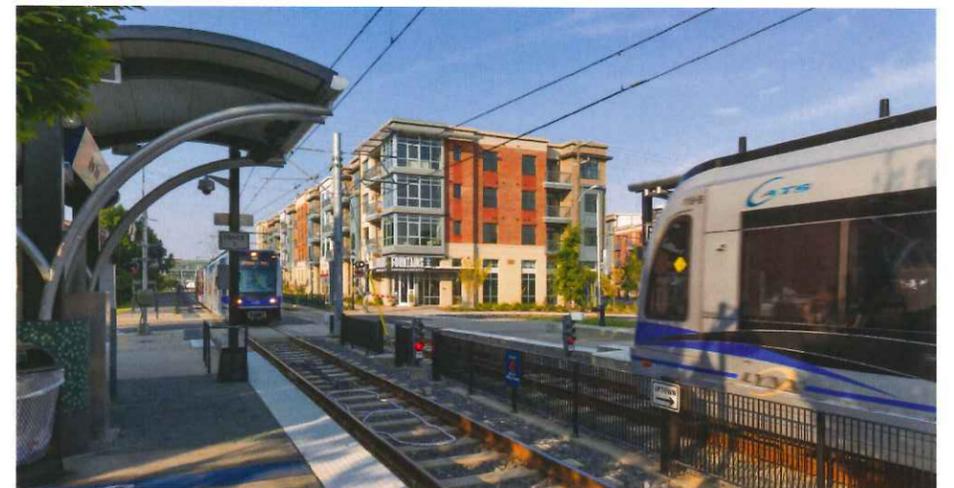
**Phillips Jackson Redevelopment District**

The two parcels with frontage along 4th Avenue North are within the Phillips Jackson Redevelopment District and shall meet the relevant standards for vacant lot new construction within the guidelines.



PROFFITT DIXON PARTNERS

MPC Case Number- 2014SP-020-001



**Purpose**

The purpose of this SP application is to provide for the development of a mixed-use development including up to 249 multifamily units within the Germantown community.

**Vision**

As per the North Nashville Community Plan's vision for a broader mixed use district for the area, the plan for Fountains Germantown will serve as a catalyst for the redevelopment of this predominantly industrial area. The goal is to create a place that enhances the Germantown community, encourages neighbors to bond, enhances the quality of life and instills a sense of belonging and community pride in residents.

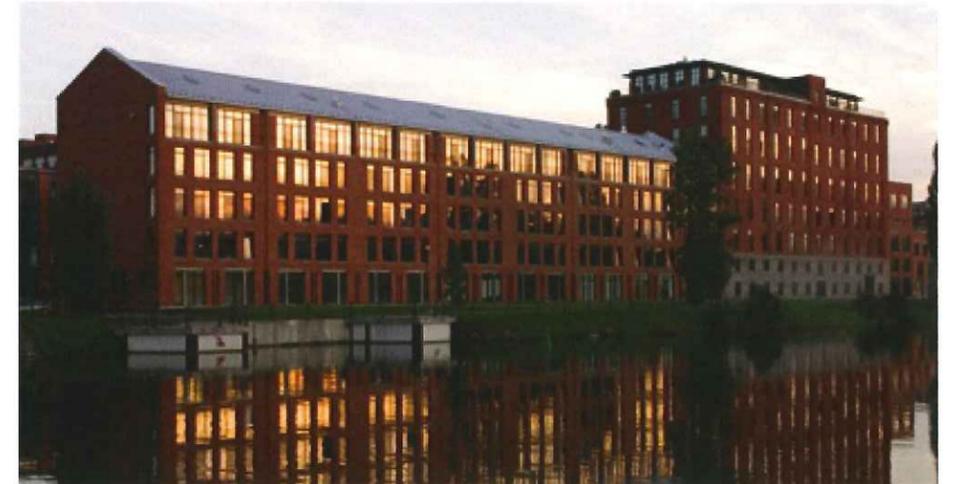
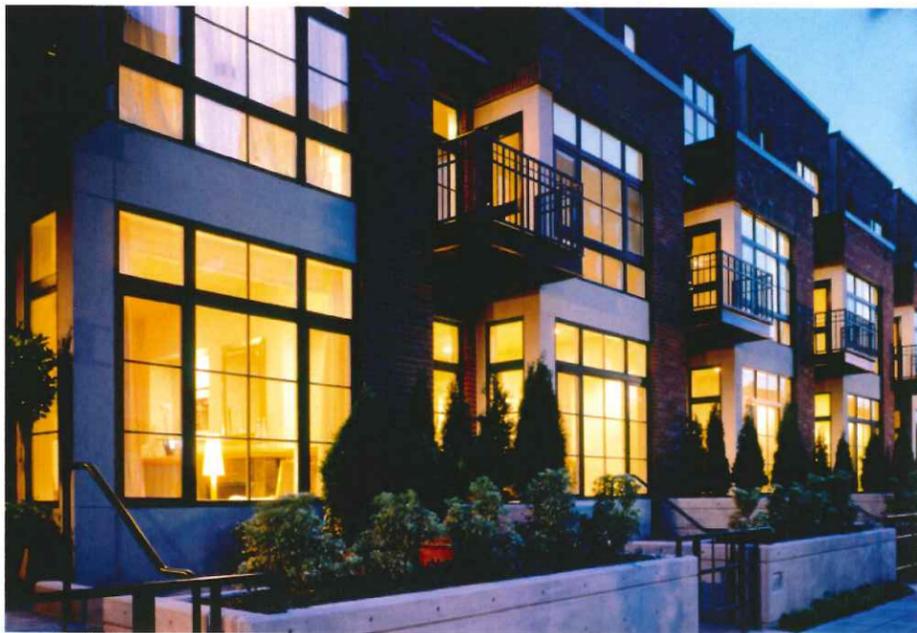
The plan focuses more intensity towards 3rd Avenue North with intensity transitioning towards 4th Avenue North respecting the residential character of the area.

The plan creates a pedestrian friendly environment surrounding the site through the inclusion of streetscape improvements, wide sidewalks, landscaping and a highly aesthetic architectural back drop that prominently addresses the street, creating a high quality public realm.

The intent for the architectural character of the building is envisioned to take queues from the industrial character of the surrounding area including the use of large windows and the strategic use of brick detailing to create a high quality, visually appealing addition to the Germantown community.

The plan includes several amenity areas including a pool courtyard and landscaped courtyard along Van Buren Street. A leasing center, fitness center, club room and other amenities will also be included.

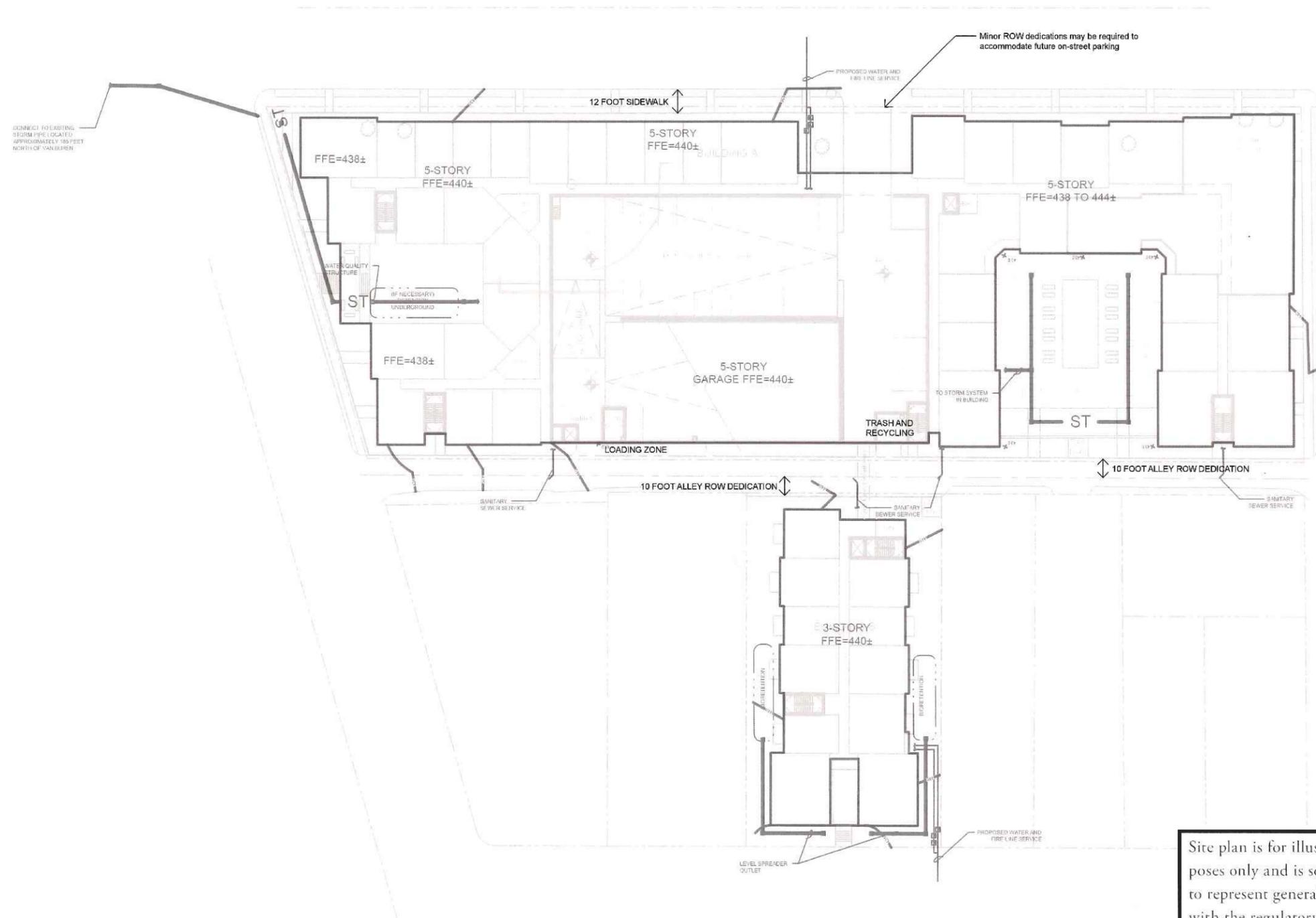




**Water & Stormwater Notes**

- All water and sewer construction shall be in accordance with specifications and standard details of the Metro Water Services.
- This drawing is for illustration purposes to indicate the basic premise of the development. The final details of the plan shall be governed by the appropriate regulations at the time of final application.
- The sheet flow system proposed on 3rd Avenue will need to be approved by Metro Stormwater / Metro Public Works. If an adequate system is not made, then a traditional storm system will be required.
- The disturbed area for this project is approximately 2.42 - acres.
- Metro Water Services shall be provided sufficient and unencumbered access in order to maintain and repair utilities in this site.
- Any excavation, fill or disturbance of the existing ground must be done in accordance with Stormwater Management Ordinance 78-840 and approved by the Metro Department of Water Services.
- According to FEMA's current flood maps (47037C0216F, dated April 20, 2001), as well as Metro's GIS information, there is no 100-year floodplain within the SP boundary.
- There are no known existing wetlands within the SP boundary
- Approval of any specific plan does not exempt any parcel shown on the plan or any development within the SP from compliance with all provisions of the Metro Zoning Code with respect to floodplain, steep slopes, unstable soils, sinkholes, rock outcroppings, streams, springs and critical lots.
- Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual (minimum driveway culvert in Metro ROW is 15" CMP).
- The sanitary sewer line shall be PVC-SDR 35 or Class 52 DIP.
- The water lines shall be class 52 ductile iron pipe.
- Individual water and sewer service lines are required for each home (where applicable).
- Water meters shall be no deeper than 24" from the top of meter to proposed finished grade unless otherwise required by the local water department.
- Metro Water Services shall be provided sufficient and unencumbered access in order to maintain and repair utilities in this site.
- The proposed stormwater management for this project includes collection of the site stormwater on the main site and directed to a stormwater detention pond as shown in this plan. The discharge from the detention pond shall then be routed to an 80% water quality unit and then discharged to the existing storm sewer located in 3rd Avenue North, just north of VanBuren Street. Extension of a public storm drainage pipe from the project site to the existing drainage pipe in 3rd Avenue

(cont'd)



Site plan is for illustrative purposes only and is solely intended to represent general compliance with the regulatory standards and standard notes herein.

North will be necessary. The proposed Stormwater management on the property fronting 4th Avenue North will include the collection of the Stormwater and directing it to a bioretention basin that is capable of treating the Stormwater to 80% water quality and then discharging the Stormwater onto the surface using a level spreader. No Stormwater detention should be necessary for this portion of the project since the existing site is currently paved and our proposed project will be decreasing the impervious area of the site. The stormwater quality and detention measures for this project will meet the requirements of the Metro Stormwater Ordinance that are in effect at the time of the final SP submittal.

**Public Works Notes**

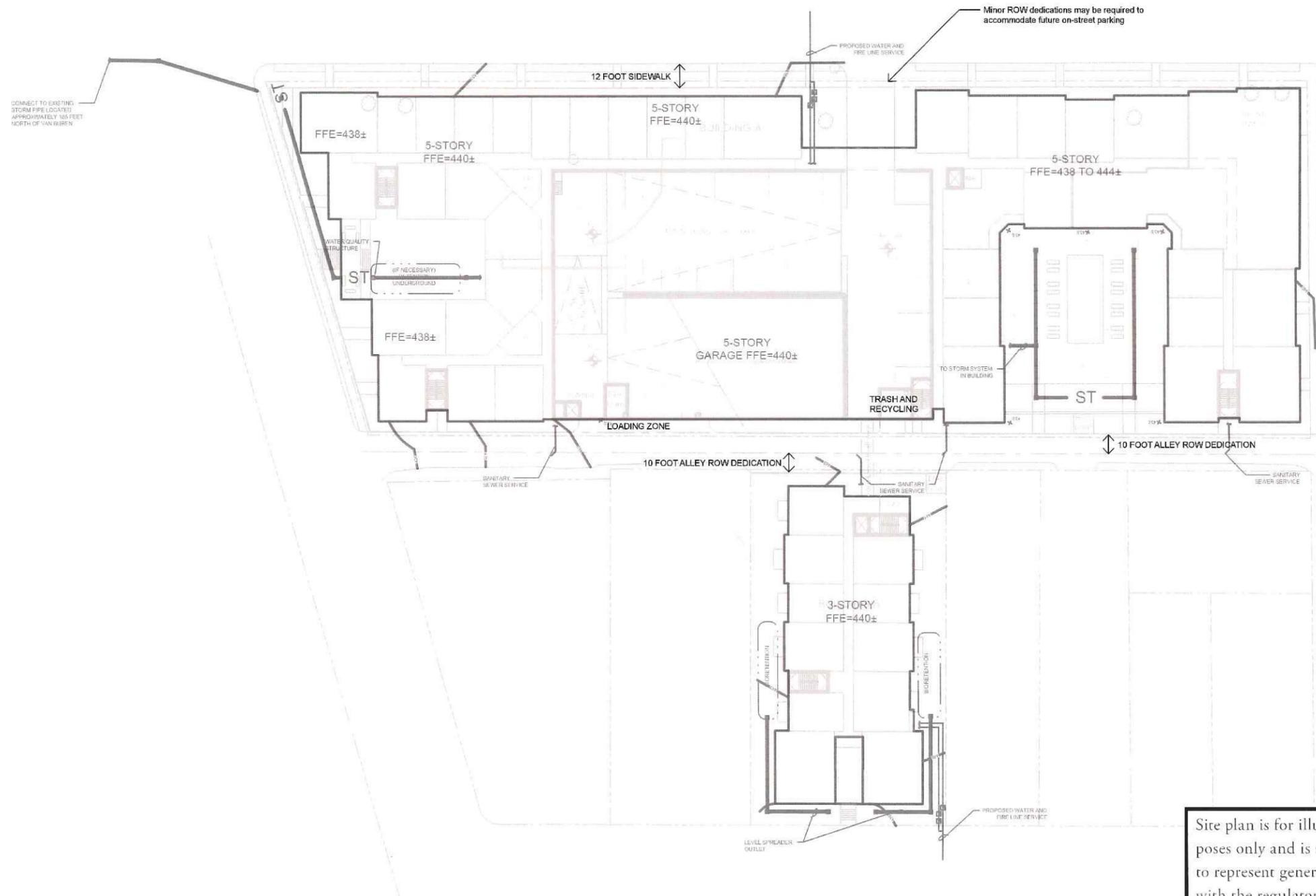
- All roadway, driveway, sidewalk, and curb construction shall conform to the requirements and specifications of Metro Public Works.
- Final construction drawings shall comply with the design regulations established by the Department of Public Works. Final design may vary based on field conditions.
- A Traffic Impact Study will be complete prior to Final SP approval. Developer shall construct any off-site roadway improvements with Final SP plan. Roadway improvements will be in accordance with the TIS findings and conditions.
- Handicap ramps shall have a maximum slope of 1:12.
- All access drives and driveways within the community shall be private.
- Sidewalks along all public streets must be located in public Right-of-way. Additional Right-of-way dedication may be required.
- Alley shall be improved to MPW ST-263. The Right-of-way dedication must be recorded prior to the issuance of building permits.
- The pedestrian walkway over the alley requires an Aerial Encroachment / Mandatory Referral that must be approved by Metro Council prior the issuance of building permits.
- Minor ROW dedications may be required on 3rd Avenue to accommodate future on-street parking

**Utility Notes**

- All proposed public utilities and services shall be installed underground.
- Trash and recycling service is to be contracted between the developer / owner and a private hauler.

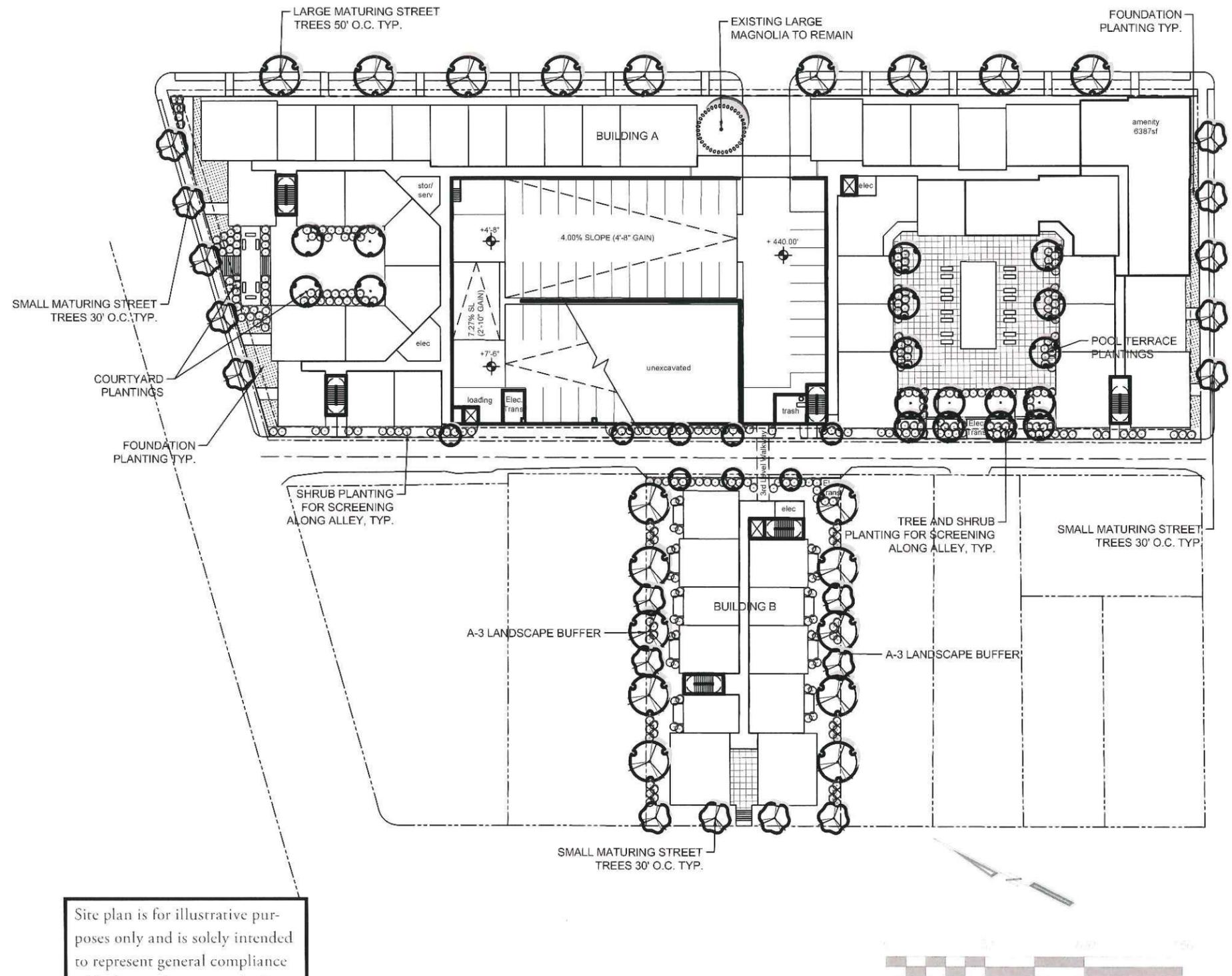
**Fire Department Notes**

- The required fire flow shall be determined by the Metro/Nashville Fire Marshall's Office prior to the issuance of a building permit.
- The requirements of the Metro Fire Marshal's Office for emergency vehicle access and adequate water supply for fire protection must be met prior to the issuance of any building permits



**Landscape Standards**

- The developer of this project shall comply with the requirements of the SP and adopted tree ordinance 2008-328 (Metro Code Chapter 17.24).
- Side yard buffers for the property shall comply with the A-3 buffer standards per 17.24.240A of the zoning code
- Street trees shall be provided along the 3rd Avenue North street frontage at a minimum spacing of fifty (50) linear feet.
- All landscaping shall be properly irrigated and maintained by a landscape contractor; if drought resistant plant material is used, irrigation shall not be required
- Where trees are planted in rows, they shall be uniform in size and shape.
- Planting size determination:
  - Trees: height shall be measured from the crown of the root ball to the top of mature growth. Spread shall be measured to the end of branching equally around the crown from the center of the trunk. Measurements are not to include any terminal growth. Single trunk trees shall be free of "v" crotches that could be points of weak limb structure or disease infestation.
  - Shrubs: height shall be measured from the crown of the root ball to the average height of the top of the plant. Spread shall be measured to the end of branching equally around the shrub mass. Measurements are not to include any terminal growth.
- All plants shall be freshly dug, sound, healthy, vigorous, well branched, free of disease, insect eggs, and larvae, and shall have adequate root systems.
- All container grown material shall be healthy, vigorous, well-rooted plants and established in the container in which they are sold. The plants shall have tops which are good quality and are in a healthy growing condition. All root bound plants shall be rejected.
- Groups of shrubs shall be placed in a continuous mulch bed with smooth continuous lines. All mulched bed edges shall be curvilinear in shape following the contour of the plant mass. Trees located within four feet of shrub beds shall share same mulch bed.
- Plant locations may be adjusted in the field as necessary to be clear of drainage swales and utilities. Finished planting beds shall be graded so as to not impede drainage away from buildings. If significant relocations are required, contractor shall contact landscape architect for resolution. Failure to make such relocations known to the owner or landscape architect will result in contractor's liability of plant materials
- Trees should remain vertical and upright for the duration of the guarantee period. Guys and strapping shall be removed after one growing season.
- The root crown to be at finished grade or no greater than a maximum of one inch higher (after settling) than finished grade.



Site plan is for illustrative purposes only and is solely intended to represent general compliance with the regulatory standards and standard notes herein.

Development Summary	
Allowable Uses	Mixed Use
Maximum Number of Units	249
Commercial FAR	0.25 Maximum
ISR	86%
3rd Avenue Build-to-Zone	0 - 10 ft. *
Van Buren and Taylor Streets Build-to-Zone	0 - 20 ft. *
Rear Setback	3 ft. minimum
Side Setback	10 ft. minimum
Height Standards	5 stories maximum in 75 feet
Access	(1) 24 ft. drive on 3rd Ave. South (1) 24 ft. drive on Alley
Parking	Per UZO parking requirements**
* Excluding pedestrian passages, garage entries and open spaces	
** It is intended that the total parking provided will be in excess of the minimum requirements of the UZO parking standards.	

Any standard not specifically addressed herein shall comply with the Mixed Use Limited Alternative (MUL-A) base zoning requirements as of the application date of this Preliminary SP.

The Executive Director of the Planning Department may modify standards within this SP insofar as the standard(s) meets the general intent of the SP guidelines and complies with the land use policy. Modifications that increase the number of uses or project density shall require approval by Metro Council.

**Permitted Uses**

All uses permitted by Mixed Use Limited Alternative (MUL-A) base zoning designation.

**Parking**

Provided parking shall meet the requirements of the UZO parking standards of the Zoning Code (17.20).

It is intended that the total parking provided will be in excess of the minimum requirements of the UZO parking standards.

Parking shall be predominantly provided within a parking area internal to the development. Additional parking spaces may be provided in the form of on-street parking along the 3rd Avenue frontage.

**Height**

The maximum height permitted for Building A shall be 5 stories within seventy-five (75) feet. Building B shall be limited to 3 stories along the 4th Avenue North street frontage.

**Architectural Standards**

Buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for every one-hundred (100) linear feet of street frontage:

- A change in building material
- A horizontal undulation in the building facade of three (3) feet or greater
- A porch, stoop or balcony
- Street facing building facades shall have a minimum of fifteen (15) percent glazing (measured as a percentage of each floors width x height).
- Ground floor residential units within Building A shall have a porch or stoop with direct access to public sidewalk for 45% of total ground floor units. Building B has joint access for both ground floor street facing units.
- Refuse collection, recycling and mechanical equipment shall be fully screened from public view by the combination of fences, walls or landscaping.

- A primary pedestrian entrance with direct access to the public sidewalk shall be provided along 3rd Avenue North.
- The minimum building frontage along all public streets shall equal eighty percent (80%) minimum, excluding pedestrian passages, garage entries and open spaces.

**Signage**

The following sign types shall be permitted: Projecting Signs, Wall Mounted Signs, Awning Signs, Ground Mounted Signs, Canopy Signs.

- Ground mounted signs shall be limited to three feet (3 ft.) in width and four feet (4 ft.) vertically.
- External Illumination: Light sources shall be directed towards sign minimizing glare to the sidewalk or adjacent properties.
- Internal Illumination: Signs may be internally illuminated but the sign background must be opaque with only graphic, text or logos being illuminated.
- Signs standards not addressed herein should meet the requirements permitted by the Mixed Use Limited Alternative (MUL-A) base zoning designation.

**Standard Notes**

- Approval of any specific plan does not exempt any parcel shown on the plan or any development within the SP from compliance with all provisions of the Metro Zoning Code with respect to floodplain, steep slopes, unstable soils, sinkholes, rock outcroppings, streams, springs and critical lots.
- All development within the boundaries of this plan will be designed based on the requirements of the Americans with Disabilities Act and the Fair Housing Act.